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PROPERTY OWNERS WITHIN 200 FT. OF SUBJECT PROPERTY  
TOWNSHIP OF MONTGOMERY, SOMERSET COUNTY, NEW JERSEY

Date: 2/10/2021

PROPERTY ID	PROPERTY LOCATION	CLASS	OWNERS NAME & ADDRESS
28005 59	990 RT 518	4A	K. T. CORPORATION 7 HOUSTEN COURT MARTINSVILLE, NJ 08836
28005 61	1290 RT 206	1	AMERICAN REALTY ASSOC%PRC.INNKKEEPER 1600 HIGHWAY 34 NEPTUNE, NJ 07753
28005 62	1290 RT 206	4A	AMERICAN REALTY ASSOC%PRC INNKEEPER 1290 RT 206 SKILLMAN, NJ 08558
28005 64	1276 RT 206	4A	MONTGOMERY 206 REALTY,LLC 1704-1714 WOODBRIDGE AVE. EDISON, NJ 08817
29002 46	1325 RT 206	4A	MONTPEN SC L.L.C. 902 CARNEGIE CTR, STE 400 PRINCETON, NJ 08542
29002 47	RT 206	4A	INTERSECTION BILLBOARDS LLC 226 KING GEORGE RD PENNINGTON, NJ 08534
29002 48	1273 RT 206	4A	NM PROPERTIES, L.L.C. 928 WEST STATE STREET TRENTON, NJ 08650
34001 80	995 RT 518	4A	1ST CONSTITUTION BANK 2650 ROUTE #130 CRANBURY, NJ 08512
35005 1	1251 RT 206	4A	1251 ROUTE 206 PRINCETON, L.L.C. P.O. BOX 385 ALLENTOWN, PA, 18105

UTILITY COMPANIES REGISTERED TO RECEIVE NOTICE

COMCAST CABLE COMPANY  
100 RANDOLPH ROAD  
SOMERSET, NJ 08873

NEW JERSEY AMERICAN WATER  
ATTN: DONNA SHORT, GIS SUPERVISOR  
1025 LAUREL OAK ROAD  
VORHEES, NJ 08043

CENTURY LINK  
ATTN: BOB O'CONNOR  
256 PAUL STREET  
BELVIDERE, NJ 07823

PUBLIC SERVICE ELECTRIC & GAS  
MANAGER-CORPORATE PROPERTIES  
80 PARK PLAZA-T6B  
NEWARK, NJ 07102

DEPARTMENT OF PUBLIC WORKS  
TOWNSHIP OF MONTGOMERY  
ATTN: ARTHUR VILLANO, SUPERINTENDANT  
2261 RT. 206  
BELLE MEAD, NJ 08502

ROADS TO CONTACT

COUNTY ROAD: COUNTY ROUTE 518 (GEORGETOWN-FRANKLIN TURNPIKE)  
STATE ROAD: STATE ROAD (ROUTE 206)

CONTACTS BY ROAD TYPE:

MUNICIPAL  
TOWNSHIP OF MONTGOMERY  
2261 RT. 206  
BELLE MEAD, NJ 08502

COUNTY  
SOMERSET COUNTY PLANNING BOARD  
P.O. BOX 3000  
SOMERVILLE, NJ 08876

STATE  
NJ DEPARTMENT OF TRANSPORTATION  
1035 PARKWAY AVE, CN600  
TRENTON, NJ 08625

OUTSIDE AGENCY APPROVALS REQUIRED

MONTGOMERY ZONING BOARD OF ADJUSTMENTS  
MONTGOMERY TWP. DEPARTMENT OF PUBLIC WORKS  
MONTGOMERY TWP. HEALTH DEPARTMENT  
SOMERSET COUNTY PLANNING BOARD  
SOMERSET COUNTY SOIL CONSERVATION DISTRICT  
DELAWARE & RARITAN CANAL COMMISSION  
NEW JERSEY DEPARTMENT OF TRANSPORTATION  
NEW JERSEY DEPT. OF ENVIRONMENTAL PROTECTION

AMENDED PRELIMINARY  
AND FINAL SITE PLAN

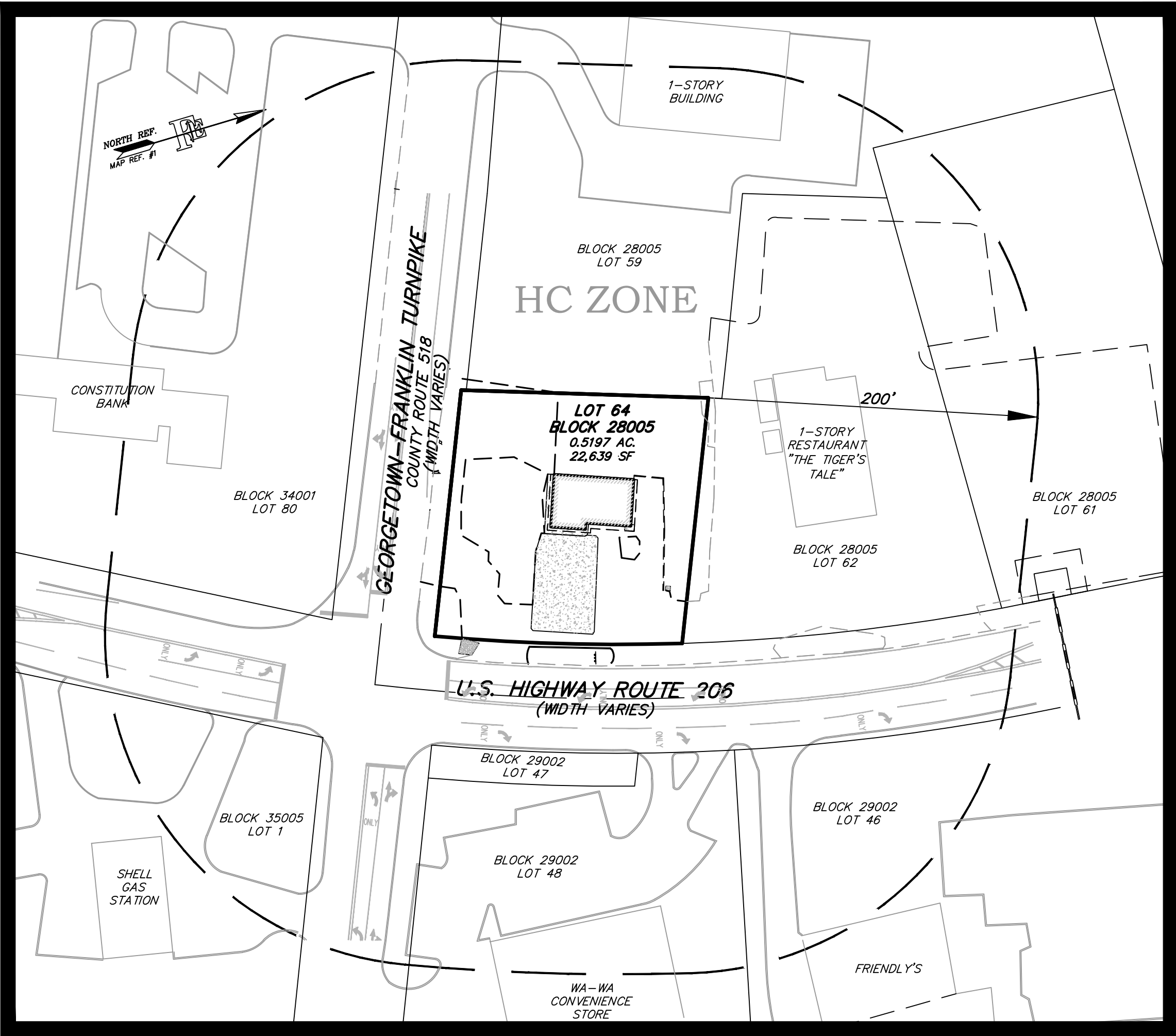
DUNKIN'

LOT 64 ~ BLOCK 28005

1276 ROUTE 206

TOWNSHIP OF MONTGOMERY  
SOMERSET COUNTY, NEW JERSEY

DATE: FEBRUARY 2021



KEY MAP  
SCALE: 1"=60'

MONTGOMERY TWP.  
TAX MAP SHEET #55

- (1) EXISTING NON-CONFORMING  
(2) VARIANCE REQUIRED  
(3) MEASURED TO CANOPY, IF APPLICABLE  
(4) MEASURED TO TOP OF CUPOLA

ZONE TABLE

HC-HIGHWAY COMMERICAL 16-4.12	REQUIRED	EXISTING	PROPOSED - PRE R.O.W. DEDICATION	PROPOSED - POST R.O.W. DEDICATION
MIN. LOT AREA	1 ACRE	0.5197 AC	0.5197 AC (1)	0.5107 AC (2)
MIN. LOT FRONTAGE	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN. LOT WIDTH	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN LOT DEPTH	150 FT	151.68 FT	151.68 FT	151.68 FT
MIN FRONT YARD (3)	50 FT	61.55 FT	47.2 FT	45.3 FT (2)
MIN SIDE YARD (3)	25 FT	38.50 FT	44.7 FT	42.8 FT
MIN REAR YARD (3)	50 FT	49.3 FT	41.3 FT (2)	41.2 FT (2)
MAX. % IMPERVIOUS LOT COVERAGE	55 %	73.1% (1) 16,954 SF	82.9% (2) 18,767 SF	84.4% (2) 18,767 SF
MAX. BUILDING HEIGHT	30 FT	< 30 FT	28.00 FT (4)	28.00 FT (4)
MAX. FLOOR AREA RATIO	0.20	0.063	0.081 1823 SF	0.082 1823 SF

VARIANCES/DESIGN WAIVERS APPROVED SEPT. 22, 2015:

- 16-6.1m(1): D-3 VARIANCE REQUESTED FOR DRIVE THROUGH WINDOW FOR RESTAURANT. DUNKIN' INTENDS TO HAVE A DRIVE THROUGH WINDOW NOT LOCATED WITHIN A SHOPPING CENTER.
- 16-6.1m(2): D-3 VARIANCE REQUESTED FOR A DRIVE THROUGH WINDOW FOR DUNKIN' RESTAURANT WITH DIRECT VEHICULAR ACCESS TO A PUBLIC STREET.
- 16-4.12.d: MINIMUM LOT AREA REQUIREMENT IS 1 ACRE WHERE THE PROPOSED LOT AREA IS 0.5107 AC.
- 16-4.12.d: MINIMUM LOT WIDTH AND FRONTAGE REQUIREMENT IS 150 FEET WHERE THE PROPOSED IS 143.46 FEET.
- 16-4.12.d: MINIMUM LOT DEPTH REQUIREMENT IS 150 FT WHERE THE PROPOSED IS 147.58 FT.
- 16-4.12.d: MINIMUM REAR YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 33.5 FT.
- 16-4.12.d: LOT COVERAGE. THE MAXIMUM ALLOWABLE IS 55% WHERE 78.5% IS PROPOSED.
- 16-4.12a.5.d VISUAL BREAKS. THE BUILDING WILL HAVE BREAKS IN COLOR AND FINISHES.
- 16-4.12a.5.d FRONT FACADE SHALL BE CONTINUED AROUND ALL VISIBLY EXPOSED SIDES. STONE FINISH WAS ADDED TO ALL SIDES OF BUILDING.
- 16-4.12.a.6.c: ROOF TREATMENT, DECORATIVE FENCING WAS ADDED TO FLAT ROOF
- 16-4.12.a.7.b WINDOWS TO OCCUPY 75% OF FIRST STORY FACADE, 42% PROPOSED
- 11.1. 16-4.12.f.4.a & 16-4.12(5): NO PARKING AREA SHALL BE PERMITTED WITHIN THE FIRST TWENTY-FIVE (25) FEET ADJACENT TO ANY STREET NOR WITHIN THE FIRST FIFTEEN (15) FEET ADJACENT TO ANY OTHER PROPERTY LINE. WHEREAS 3.0' IS PROPOSED ALONG COUNTY ROUTE 518 AND 9.1' ALONG STATE HIGHWAY ROUTE 206.
- 16.4.12.h: OFF STREET LOADING SPACE REQUIRED AT SIDE OR REAR OF BUILDING. NO LOADING SPACE PROVIDED.
- 16-4.12.i (1) FREE STANDING SIGN PERMITTED, 1 SIGN PROPOSED AND 2 MENU BOARDS PROPOSED.
- 16-5.4.b: THE AVERAGE LIGHT INTENSITY SHALL BE NO MORE THAN 1.0 FC WHERE 2.1 FC IS PROVIDED.

REQUESTED VARIANCES/DESIGN WAIVERS

- 16-4.12.d: MINIMUM FRONT YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 45.3 FT.
- 16-4.12.d: MINIMUM REAR YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 41.2 FT.
- 16-4.12.d: LOT COVERAGE. THE MAXIMUM ALLOWABLE IS 55% WHERE 84.4% IS PROPOSED.
- 16-4.12.a.5.c BUILDINGS WITH 2 FRONTAGES SHALL HAVE 2 FACADES, DRIVE-UP WINDOW PROPOSED ON 518 FRONTAGE
- 16-4.12.f.3 REQUIRES 45% OF LOT TO BE LANDSCAPED, 2,865SF OR 12.9% PROPOSED
- 16-4.12.f.4.a & 16-4.12.f.5: NO PARKING AREA SHALL BE PERMITTED WITHIN THE FIRST TWENTY-FIVE (25) FEET ADJACENT TO ANY STREET NOR WITHIN THE FIRST FIFTEEN (15) FEET ADJACENT TO ANY OTHER PROPERTY LINE. WHEREAS 4.0' IS PROPOSED ALONG COUNTY ROUTE 518 AND 9.1' ALONG STATE HIGHWAY ROUTE 206.
- 16-4.12.f.5 REQUIRES A 15' SETBACK FROM THE PROPERTY LINE FOR ALL STRUCTURES. PROPOSED PARKING, DRIVEWAY AND RETAINING WALL ARE CLOSER TO PROPERTY LINE. FACE OF RETAINING WALL LOCATED ALONG WESTERLY PROPERTY LINE.
- 16-5.4.b: THE AVERAGE LIGHT INTENSITY SHALL BE NO MORE THAN 1.0 FC WHERE 1.1 FC IS PROVIDED.
- 16-5.6.d.15 STREET TREES REQ'D. AT 50' INTERVALS, 2 STREET TREES PROPOSED, A VARIANCE IS REQUIRED FOR THIS ITEM.
- 16-5.13.f.5 ON-SITE DIRECTIONAL SIGNS, PARKING SIGNS, WARNING SIGNS AND OTHER SIMILAR SIGNS SHALL BE PERMITTED, PROVIDED THEY DO NOT EXCEED TWO SQUARE FEET IN AREA AND DO NOT DISPLAY ANY TYPE OF ADVERTISING. DIRECTIONAL SIGNS PROPOSED 2 SF REQUIRED, GREATER THAN 2 SF PROPOSED, SIGN SIZES VARY.

AMENDED PRELIMINARY  
AND FINAL SITE PLAN

FOR

DUNKIN'

LOT 64 ~ BLOCK 28005

TOWNSHIP OF MONTGOMERY

SOMERSET COUNTY, NEW JERSEY

PLANS PREPARED BY

→ Ferriero Engineering, Inc

180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
CERTIFICATE OF AUTHORIZATION 240A27935400

2/25/2021

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER

DATE

FEI No. 141046

NO. GE32978

SHEET 1 OF 13

THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL AND MAY NOT BE USED FOR CONSTRUCTION UNTIL ALL PERMITS HAVE BEEN OBTAINED

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NOTE:  
ALL CONSTRUCTION SHALL CONFORM TO THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

THIS SITE PLAN HAS BEEN APPROVED  
BY THE ZONING BOARD OF ADJUSTMENT  
OF MONTGOMERY TOWNSHIP  
ON \_\_\_\_\_ DATE \_\_\_\_\_

CHAIRMAN \_\_\_\_\_ DATE \_\_\_\_\_

SECRETARY \_\_\_\_\_ DATE \_\_\_\_\_

TOWN ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

SOMERSET COUNTY  
ACCEPTANCE STAMP

THESE PLANS ARE NOT ACCEPTABLE FOR  
CONSTRUCTION UNLESS THIS BLOCK IS STAMPED  
AND SIGNED BY A STAFF MEMBER OF THE  
SOMERSET COUNTY ENGINEERING DIVISION.

ATTORNEY:  
JEFFREY B. LEHRER, ESQ.  
15 MOUNTAIN BOULEVARD  
WARREN, NJ 07059  
908-757-8039

ARCHITECT:  
GARY KLIESCH  
GK+A ARCHITECTS, PC  
36 AMES AVENUE  
RUTHERFORD, NJ 07070  
201-896-0333

TRAFFIC ENGINEER:  
CRAIG PEREGOY, P.E.  
DYNAMIC ENGINEERING  
245 MAIN STREET, STE. 110  
CHESTER, NJ 07930  
908-879-9229

OWNER/APPLICANT:  
TONY NADAR  
MONTGOMERY 206 REALTY  
1714 WOODBRIDGE AVENUE  
EDISON, NJ 08817  
732-572-5000



GENERAL NOTES — APPLIES TO ALL SHEETS

1. THIS PROPERTY IS LOCATED IN THE HC ZONE AS DEPICTED ON THE MONTGOMERY TOWNSHIP TAX MAP SHEET 55.
2. BOUNDARY AND EXISTING CONDITIONS FROM SURVEY ENTITLED, "LOCATION AND TOPOGRAPHIC SURVEY OF 1276 ROUTE 206, LOT 64 IN BLOCK 28001" DATED 4/21/2006, PREPARED BY BERTIN ENGINEERING ASSOCIATES, INC., GLEN ROCK, NJ.
3. PROPOSED BUILDING FOOTPRINT FROM ARCHITECTURAL PLANS ENTITLED PROPOSED DUNKIN', DATED NOVEMBER 24, 2020, PREPARED BY GK+A ARCHITECTS, P.C., RUTHERFORD, NJ.
4. EXISTING SANITARY SEWER INFORMATION TAKEN FROM A PLAN ENTITLED "SANITARY SEWER EXTENSION BLOCK 29002, LOT 46 & ROUTE 206 CONSTRUCTION PLAN & DETAILS" DATED SEPTEMBER 16, 2005, LAST REVISED SEPTEMBER 24, 2009, PREPARED BY MENLO ENGINEERING ASSOCIATES, INC., HIGHLAND PARK, NJ.
5. SOIL BOUNDARIES AND IDENTIFICATIONS DEPICTED ON THE PLANS TAKEN FROM THE USDA WEB SOIL SURVEY.
6. THIS PROJECT PROPOSES <1 ACRE OF DISTURBANCE AND <0.25 ACRES OF NEW IMPERVIOUS SURFACES, AND IS NOT CONSIDERED AN ADJUNCT DEVELOPMENT.
7. ALL CONSTRUCTION ON THIS PROJECT SHALL BE IN ACCORDANCE WITH TOWN STANDARDS AND INSPECTIONS AND AS GOVERNED BY THE 2019 U.S. CUSTOMARY ENGLISH STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, OF THE NEW JERSEY DEPARTMENT OF TRANSPORTATION.
8. CONTRACTOR TO NOTIFY TOWNSHIP ENGINEER 48 HOURS IN ADVANCE OF ANY LAND DISTURBANCE.
9. THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL. THESE PLANS SHALL NOT BE CONSIDERED CONSTRUCTION DOCUMENTS UNTIL ALL APPLICABLE PERMITS HAVE BEEN OBTAINED BY THE DEVELOPER. ALL CONDITIONS OF THE MUNICIPAL RESOLUTION HAVE BEEN SATISFIED, AND THE PLANS HAVE BEEN SIGNED BY THE BOARD CHAIR, BOARD SECRETARY AND BOARD ENGINEER.
10. THESE PLANS ARE NOT VALID UNLESS SIGNED AND SEALED BY THE LICENSED PROFESSIONAL SHOWN HEREON.
11. ALL PROPOSED UTILITIES SHALL BE INSTALLED UNDERGROUND. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE EXISTING UTILITIES AND COORDINATING THE CONNECTION WITH MUNICIPALITY AND UTILITY COMPANIES.
12. THERE SHALL BE NO BURYING OF DEBRIS, TREES, STUMPS OR CONSTRUCTION MATERIAL ONSITE.
13. THE PROPOSED STRUCTURE IS TO BE SERVED BY PUBLIC WATER & SEWER.
14. THERE ARE NO FRESHWATER WETLANDS AND WATERS PRESENT ON THE SUBJECT PROPERTY PER NJDEP L.O.I. FILE No.:1813-13-00021 ACTIVITY NUMBER FWM30001
15. THIS PROJECT IS NOT SUBJECT TO ANY FLOOD HAZARD AREAS OR SPECIAL WATER RESOURCE PROTECTION AREAS.
16. THERE ARE NO CATEGORY ONE STREAMS AFFECTED BY THE PROPOSED CONSTRUCTION PER NJDEP GEO WEB MAPPING SOFTWARE.
17. OWNER/APPLICANT/BUILDER/CONTRACTOR TO NOTIFY DESIGN ENGINEER IMMEDIATELY IF ANY PLAN INCONSISTENCIES EXIST.
18. NO TOPSOIL SHALL BE REMOVED FROM THE SITE OR USED AS SPOIL WITHOUT FIRST OBTAINING A PERMIT FROM THE TOWNSHIP.
19. OUTSIDE AGENCY APPROVALS ARE REQUIRED FROM THE SOMERSET COUNTY SOIL CONSERVATION DISTRICT AND THE SOMERSET COUNTY PLANNING BOARD.
20. CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY CODES. APPLICABLE SAFETY CODES SHALL BE THE LATEST EDITION INCLUDING ANY AND ALL AMENDMENTS, REVISIONS AND ADDITIONS THERETO OF THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION'S "OCCUPATIONAL SAFETY AND HEALTH STANDARDS" (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION" OF THE STATE OF NEW JERSEY, DEPARTMENT OF LABOR AND INDUSTRY, BUREAU OF ENGINEERING AND SAFETY, "CONSTRUCTION SAFETY CODE," AND "MAINTENANCE, CONSTRUCTION AND DEMOLITION," AND "BUILDING CODE."
21. ALL SOIL TESTING FOR STRUCTURAL COMPONENTS TO BE DONE BY OTHERS. THIS INCLUDES BUT IS NOT LIMITED TO STRUCTURAL STABILITY, WATER TABLE ELEVATION, FOOTING DESIGN, PRESENCE OR ABSENCE OF BURIED OR UNSUITABLE MATERIAL, AND DEPTH TO ROCK. SITE DESIGN ENGINEER ACCEPTS NO LIABILITY FOR LOSS TIME OR ADDITIONAL EXPENSE DUE TO CHANGES OCCURRING FROM AFORESAID CONDITIONS.
22. BUILDER/OWNER/CONTRACTOR OR AUTHORIZED REPRESENTATIVE TO APPROVE PROPOSED BUILDING ELEVATIONS AND LOCATION PRIOR TO ANY EXCAVATION. IF CHANGES ARE REQUESTED, THE SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOT TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM FIELD MODIFICATIONS.
23. THE OWNER, BY AUTHORIZING FERRIERO ENGINEERING, INC. TO PERFORM CONSTRUCTION STAKE OUT OF ANY COMPONENT OF THE WORK HAS ACKNOWLEDGED THEIR REVIEW AND APPROVAL OF THE LOCATION AND ELEVATION OF ALL IMPROVEMENTS SHOWN.
24. THE STANDARD OF CARE FOR ALL PROFESSIONAL ENGINEERING AND RELATED SERVICES PERFORMED OR FURNISHED BY FEI UNDER THIS AGREEMENT WILL BE THE CARE AND SKILL ORDINARILY USED BY MEMBERS OF THE SUBJECT PROFESSION PRACTICING UNDER SIMILAR CIRCUMSTANCES AT THE SAME TIME AND IN THE SAME LOCALITY. FEI MAKES NO WARRANTIES, EXPRESSED OR IMPLIED, UNDER THIS AGREEMENT OR OTHERWISE, IN CONNECTION WITH THE SERVICES PROVIDED.
25. FERRIERO ENGINEERING, INC. MAY USE ALL INFORMATION CONTAINED IN OUTSIDE CONSULTANTS' REPORTS, DATA, SURVEYS AND INSTRUCTIONS FROM THE CONSULTANTS THAT HAVE BEEN HIRED BY THE CLIENT TO WORK ON THIS PROJECT IN PERFORMING ITS SERVICES AND IS ENTITLED TO RELY UPON THE ACCURACY AND COMPLETENESS THEREOF. FEI SHALL NOT BE HELD RESPONSIBLE FOR ANY ERRORS OR OMISSIONS THAT MAY ARISE AS A RESULT OF ERRONEOUS OR INCOMPLETE INFORMATION PROVIDED BY THE CLIENT AND/OR THE CLIENT'S CONSULTANTS AND/OR CONTRACTORS.
26. FEI SHALL NOT AT ANY TIME SUPERVISE, DIRECT, OR HAVE CONTROL OVER ANY CONTRACTORS WORK, NOR SHALL FEI HAVE AUTHORITY OVER OR RESPONSIBILITY FOR THE MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES OF CONSTRUCTION BY ANY CONTRACTOR, FOR SAFETY PRECAUTIONS AND PROGRAMS OF INCIDENT TO A CONTRACTORS WORK PROGRESS, NOR FOR ANY FAILURE OF ANY CONTRACTOR TO COMPLY WITH LAWS AND REGULATIONS APPLICABLE TO CONTRACTORS WORK. THE CLIENT WAIVES ANY CLAIMS AGAINST FERRIERO ENGINEERING, INC. ARISING FROM ERRORS, OMISSIONS, OR INACCURACIES IN DOCUMENTS OR OTHER INFORMATION PROVIDED TO US BY OTHERS.
27. FEI SHALL NOT BE RESPONSIBLE FOR THE ACTS OR OMISSIONS OF ANY CONTRACTOR, SUBCONTRACTOR, SUPPLIER, OR OF ANY CONTRACTORS AGENTS OR EMPLOYEES OR ANY OTHER PERSONS AT THE PROJECT SITE OR OTHERWISE FURNISHING OR PERFORMING ANY CONSTRUCTION WORK; OR FOR ANY DECISION MADE ON INTERPRETATIONS OR CLARIFICATIONS OF THE CONSTRUCTION CONTRACT GIVEN BY CLIENT WITHOUT THE CONSULTATION AND ADVICE OF ENGINEER.
28. ALL DESIGN DOCUMENTS PREPARED OR FURNISHED BY FEI ARE INSTRUMENTS OF SERVICE, AND FEI RETAINS AN OWNERSHIP AND PROPERTY INTEREST (INCLUDING COPYRIGHT AND THE RIGHT TO REUSE) IN SUCH DOCUMENTS, WHETHER OR NOT THE PROJECT IS COMPLETED. THE CLIENT MAY OBTAIN COPIES FOR INFORMATION AND REFERENCE IN CONNECTION WITH THE PROJECT PROVIDED THAT FEES FOR SERVICES RENDERED HAVE BEEN PAID IN FULL. IN ADDITION, THE REASONABLE COSTS OF RESEARCH AND REPRODUCTION FOR COPIES OR RECORDS SHALL BE PAID.
29. ALL DIMENSIONS SHOWN ON THE PLANS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER IF ANY DISCREPANCIES EXIST PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN CHANGES. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
30. SOLID WASTE TO BE DISPOSED OF BY CONTRACTOR IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL REGULATIONS.
31. THE EXISTING UNDERGROUND UTILITY INFORMATION SHOWN ON THESE PLANS IS APPROXIMATE AND IS NOT GUARANTEED TO ACCURACY OR COMPLETENESS. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL INFORMATION. THE CONTRACTOR SHALL UNDERGROUND UTILITIES (800) 272-1000 FOR MARKOUT PRIOR TO EXCAVATION. TEST PITS SHALL BE EXCAVATED BY THE CONTRACTOR AT ALL CROSSINGS PRIOR TO CONSTRUCTION TO ASCERTAIN LOCATIONS, ELEVATIONS, MATERIALS, AND SIZES. TEST PIT DATA SHALL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION TO PERMIT ADJUSTMENTS AS REQUIRED.
32. ALL EXCAVATED UNSUITABLE MATERIAL MUST BE TRANSPORTED TO AN APPROVED DISPOSAL LOCATION.
33. CONTRACTOR IS RESPONSIBLE FOR SHIELDING EXCAVATION AND SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT OSHA STANDARDS, AS WELL AS ADDITIONAL PROVISIONS TO ASSURE STABILITY OF CONTIGUOUS STRUCTURES, AS FIELD CONDITIONS DICTATE.
34. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE REQUIREMENTS AND STANDARDS OF THE LOCAL GOVERNING AUTHORITY. THE LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL MAINTAIN RECORDS. IT SHALL BE THE CONTRACTORS FULL RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES TO LOCATE THEIR FACILITIES PRIOR TO CONSTRUCTION.
35. STRUCTURES AND IMPROVEMENTS ON ADJACENT PROPERTIES ARE APPROXIMATE PER AERIAL PHOTOGRAPHY.
36. THE CONTRACTOR IS RESPONSIBLE FOR VERIFICATION OF EXISTING TOPOGRAPHIC INFORMATION AND UTILITY INVERT ELEVATIONS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. CONTRACTOR TO ENSURE 0.75% MINIMUM SLOPE AGAINST ALL ISLANDS, GUTTERS, AND CURBS; 1.0% ON ALL CONCRETE SURFACES; AND 1.5% MINIMUM ON ASPHALT, TO PREVENT PONDING. ANY DISCREPANCIES THAT MAY AFFECT THE PUBLIC SAFETY OR PROJECT COST MUST BE IDENTIFIED TO THE ENGINEER IN WRITING IMMEDIATELY. PROCEEDING WITH CONSTRUCTION WITHOUT NOTIFICATION IS DONE AT THE CONTRACTORS OWN RISK.
37. PROPOSED TOP OF CURB ELEVATIONS ARE GENERALLY 6" ABOVE EXISTING LOCAL ASPHALT GRADE UNLESS OTHERWISE NOTED. FIELD ADJUST TO CREATE A MINIMUM OF 0.75% GUTTER GRADE. ASH CURE CURB SURFACE. ENGINEER TO APPROVE FINAL CURBING CUT SHEETS PRIOR TO INSTALLATION.
38. SANITARY LATERALS SHALL BE 40" OR PVC SDR 35 UNLESS OTHERWISE INDICATED OTHERWISE.
39. AN AS-BUILT PLAN CERTIFIED BY A PROFESSIONAL LAND SURVEYOR SHALL BE SUPPLIED AT THE COMPLETION OF CONSTRUCTION.
40. A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.
41. THIS PROJECT WILL REQUIRE 820 CYL OF FILL, CONTRACTOR TO VERIFY, PRIOR TO IMPORTING ANY MATERIAL, APPROVAL FROM MONTGOMERY TOWNSHIP PLANNING BOARD IS REQUIRED.

\*DEMOLITION NOTES:

1. CONTRACTOR TO OBTAIN DEMOLITION PERMIT FROM MONTGOMERY TOWNSHIP BUILDING DEPARTMENT.
2. CONTRACTOR TO CONDUCT DEMOLITION TO MINIMIZE INTERFERENCE WITH ADJACENT STRUCTURES.
3. CONDUCT OPERATIONS WITH MINIMUM INTERFERENCE TO PUBLIC OR PRIVATE ACCESS. MAINTAIN ACCESS TO THE SITE AT ALL TIME.
4. REMOVE ALL DEBRIS, RUBBISH, AND OTHER MATERIALS RESULTING FROM DEMOLITION OPERATIONS FROM SITE.
5. REMOVE ALL STRUCTURE FOUNDATIONS (WALL FOOTINGS, BUILDING FOOTINGS) COMPLETELY & DISPOSE OF OFF-SITE PER LOCAL, STATE AND FEDERAL REGULATIONS. EXCAVATION AREA FOR EXISTING FOOTINGS SHALL BE BACKFILLED WITH MATERIAL APPROVED BY THE ENGINEER.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND IDENTIFYING ALL ON-SITE UNDERGROUND UTILITIES AND DRAINAGE LINES PRIOR TO COMMENCEMENT OF WORK. THIS SHALL BE IN ADDITION TO THE MARK-OUT RESULTING FROM (1-800-272-1000) NJ ONE-CALL MARK-OUT. IF UTILITIES ARE DISCOVERED FROM THE MARK-OUT THAT ARE NOT SHOWN ON THE PLAN, CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY BEFORE PROCEEDING WITH TEST PIT EXCAVATION WORK.
7. CONTRACTOR SHALL PROVIDE TEMPORARY DRAINAGE AS NEEDED TO KEEP THE SITE FREE OF STANDING WATER & MUD.

NOTE:

1. REMOVE ALL EXISTING SITE LIGHTS AND SIGN POSTS.
2. NO TREES ARE TO BE REMOVED FROM LOT 59.
3. EXISTING TREE ROW AND BUSHES ALONG THE NORTHERN, WESTERN AND SOUTHERN PROPERTY LINES CONTAIN TREES HAVING A CALIPER OF SIX (6) INCHES OR MORE MEASURED FOUR AND ONE HALF (4-1/2) INCHES ABOVE GROUND LEVEL.
4. UNDERGROUND STORAGE TANK REMOVAL & ABANDONMENT SHALL BE CONDUCTED IN ACCORDANCE WITH APPROVED CLOSURE PLAN PREPARED BY OTHERS.
5. NO CONSTRUCTION DEBRIS/MATERIALS ARE TO BE BURIED ONSITE. ALL EXCAVATED CONSTRUCTION MATERIALS MUST BE HAULED AWAY TO AN APPROVED REFUSE SITE PER LOCAL, STATE AND FEDERAL REGULATIONS.
6. EXISTING ASPHALT AND OTHER RECYCLABLE MATERIALS REMOVED DURING DEMOLITION SHALL BE DELIVERED TO AN APPROVED FACILITY FOR RECYCLING.

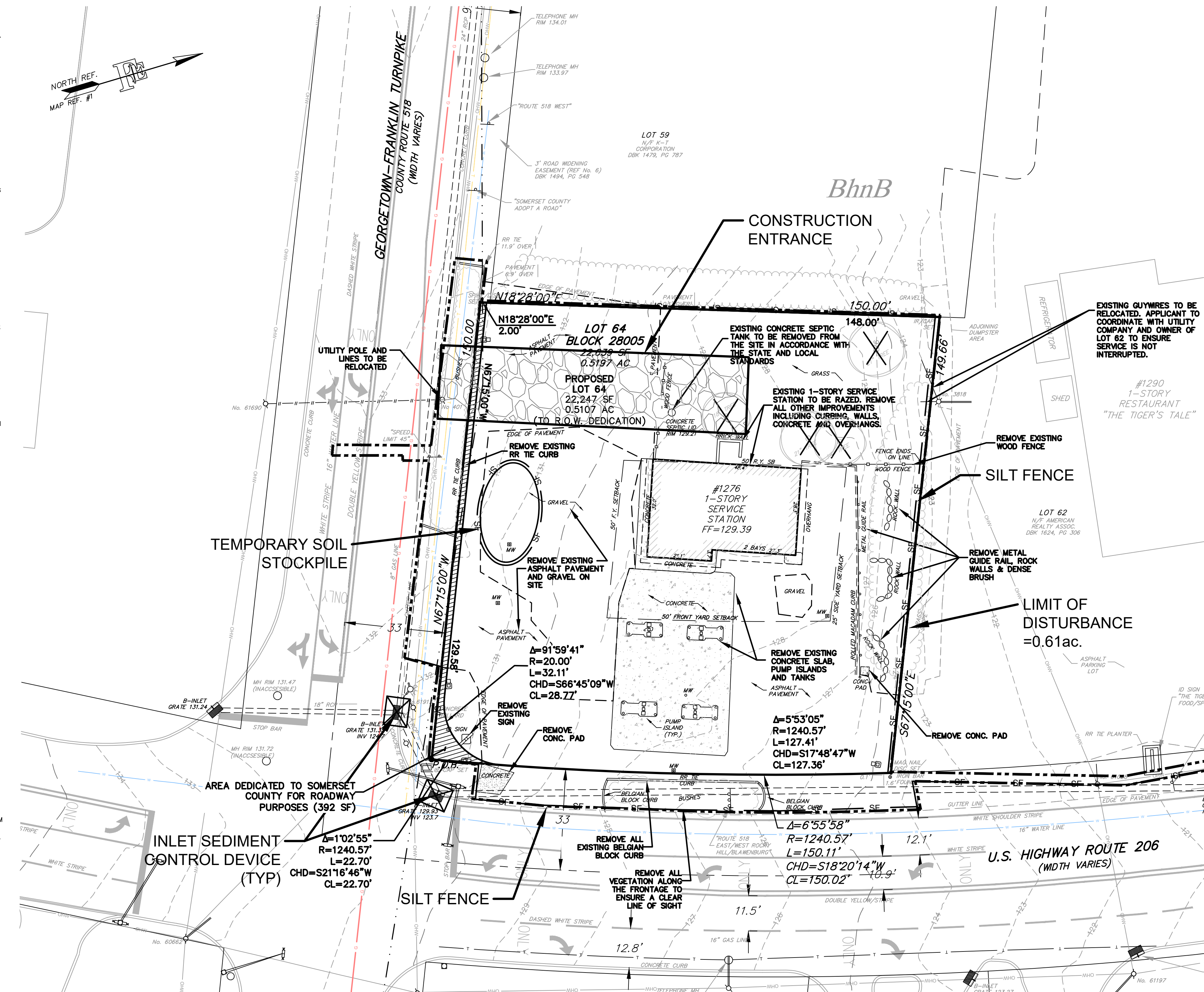
SOILS CLASSIFICATION AND ENGINEERING SOIL PROPERTIES

MAP SYMBOL	SOIL SERIES	SLOPE	HYDROLOGIC SOIL GROUP
BhnB	BIRDSBORO SILT LOAM	2-6%	B

REFERENCE:  
ENGINEERING SOIL PROPERTIES, AND CLASSIFICATIONS, TAKEN FROM THE UNITED STATES DEPARTMENT OF AGRICULTURE WEB SOIL SURVEY, PREPARED BY THE USDA NATURAL RESOURCES CONSERVATION SERVICE.

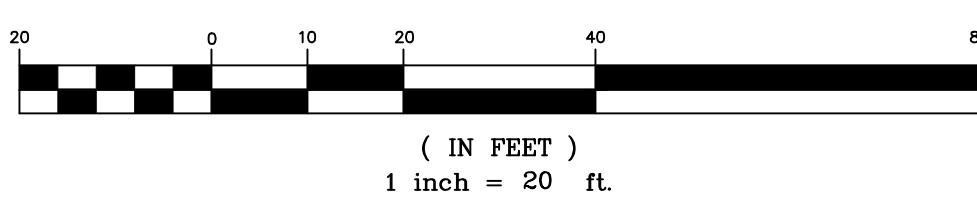
EXISTING UTILITY NOTE:

THE EXISTING UNDERGROUND UTILITY INFORMATION SHOWN ON THESE PLANS IS APPROXIMATE AND IS NOT GUARANTEED AS TO ACCURACY OR COMPLETENESS. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL INFORMATION. THE CONTRACTOR SHALL CALL UNDERGROUND UTILITIES 1 (800) 272-1000 FOR MARKOUT PRIOR TO EXCAVATION. TEST PITS SHALL BE EXCAVATED BY THE CONTRACTOR AT ALL CROSSINGS PRIOR TO CONSTRUCTION TO ASCERTAIN EXISTING LOCATIONS, ELEVATIONS, MATERIALS, AND SIZES. TEST PIT DATA SHALL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION TO PERMIT ADJUSTMENTS AS REQUIRED.



DEMOLITION PLAN

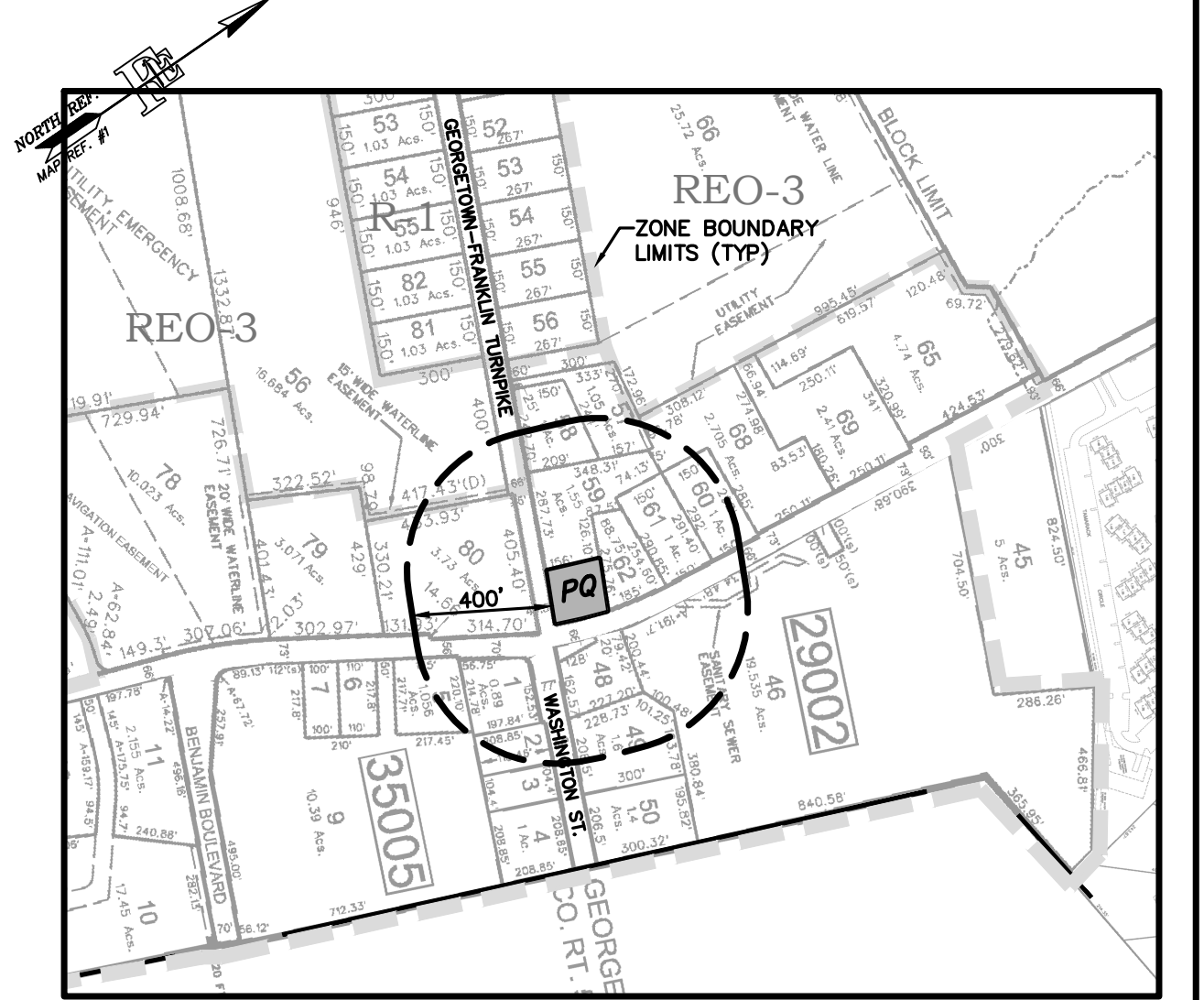
GRAPHIC SCALE



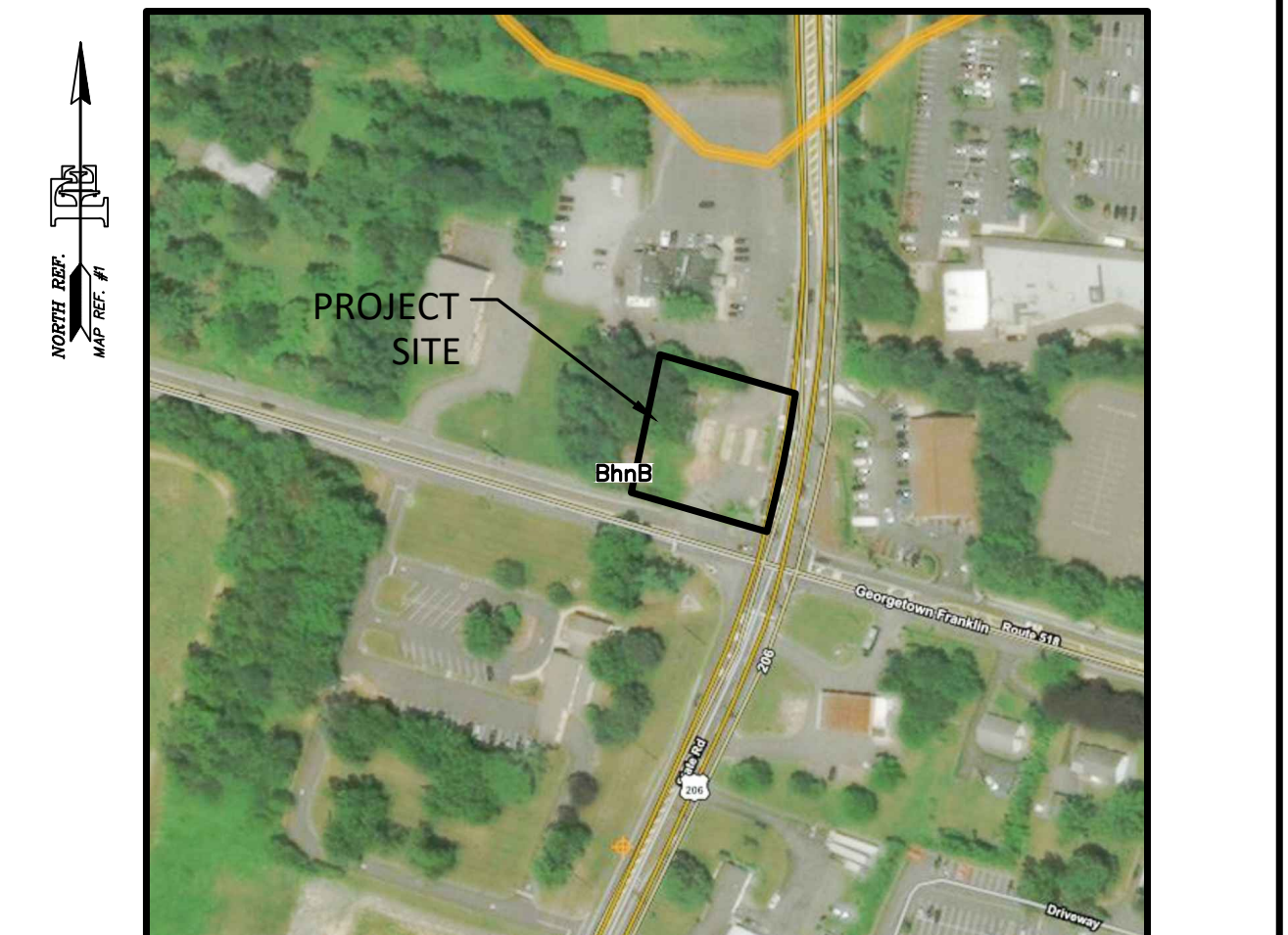
EXISTING IMPERVIOUS AREA CALCULATIONS	
IMPROVEMENTS	AREAS
EX. SERVICE STATION BUILDING W/ OVERHANG	1,718 SF
EXISTING PAVEMENT	8,930 SF
EXISTING CONCRETE/WALKS	2,363 SF
EXISTING GRAVEL AREAS	3,543 SF
TOTAL EXISTING IMPERVIOUS	16,554 SF (73.1%)

ZONE TABLE				
HC-HIGHWAY COMMERICAL 16-4.12	REQUIRED	EXISTING	PROPOSED - PRE R.O.W. DEDICATION	PROPOSED - POST R.O.W. DEDICATION
MIN. LOT AREA	1 ACRE	0.5197 AC	0.5197 AC (1)	0.5107 AC (2)
MIN. LOT FRONTAGE	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN. LOT WIDTH	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN. LOT DEPTH	150 FT	151.68 FT	151.68 FT	151.68 FT
MIN FRONT YARD (3)	50 FT	61.55 FT	47.2 FT	45.3 FT (2)
MIN SIDE YARD (3)	25 FT	38.50 FT	44.7 FT	42.8 FT
MIN REAR YARD (3)	50 FT	49.3 FT	41.3 FT (2)	41.2 FT (2)
MAX. % IMPERVIOUS LOT COVERAGE	55 %	73.1% (1) 16,554 SF	82.9% (2) 18,767 SF	84.4% (2) 18,767 SF
MAX. BUILDING HEIGHT	30 FT	< 30 FT	28.00 FT (4)	28.00 FT (4)
MAX. FLOOR AREA RATIO	0.20	0.063	0.081 1823 SF	0.082 1823 SF

- (1) EXISTING NON-CONFORMING
- (2) VARIANCE REQUIRED
- (3) MEASURED TO CANOPY, IF APPLICABLE
- (4) MEASURED TO TOP OF CUPOLA



VICINITY MAP  
SCALE: 1" = 500'



SOILS MAP  
SCALE: 1" = 200'

LEGEND

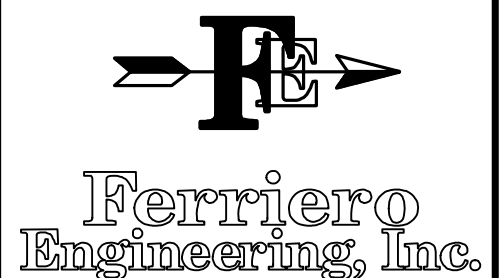
- 540 — EXISTING CONTOUR LINE
- 520 — PROPOSED CONTOUR LINE
- 516.3 — EXISTING SPOT GRADE
- + 640.00 — PROPOSED SPOT GRADE
- EXISTING CURB LINE
- PROPOSED CURB LINE
- EXISTING INLET
- PROPOSED INLET
- EXISTING STORM/SAN PIPE
- PROPOSED STORM SEWER
- EXISTING SITE LIGHT
- EXISTING GAS LINE
- EXISTING WATER LINE
- EXISTING OVERHEAD WIRE
- EXISTING UTILITY POLE
- EXISTING STONE WALL
- EXISTING FENCE
- PROPOSED CONCRETE
- EXISTING SITE LIGHT
- EXISTING TREE TO BE REMOVED
- INLET SEDIMENT CONTROL DEVICE

MAP REFERENCES:

1. MAP ENTITLED "SURVEY OF PROPERTY, 1276 ROUTE 206, TOWNSHIP OF MONTGOMERY, SOMERSET COUNTY, NEW JERSEY PREPARED BY FERRIERO ENGINEERING, INC. DATED SEPTEMBER 6, 2016.
2. TOWNSHIP OF MONTGOMERY TAX MAP No. 55.

THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL AND MAY NOT BE USED FOR CONSTRUCTION UNTIL ALL PERMITS HAVE BEEN OBTAINED

NO.	DATE	REVISION
1	5/20/2021	PER TMP COMPLETENESS REVIEW
DRAWN BY: SCALE: 1" = 20'		
AMH	FIELD BOOK:	
CHKD BY: FILE:		
PWF	41046/DWG/BASEMAPS/BASEMAPS-9-2020	
PAUL W. FERRIERO N.J. PROFESSIONAL ENGINEER NO. GE32978		



180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 240A27935400

DEMOLITION PLAN

DUNKIN'

LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET	
2 OF 13	
DATE: 2/25/2021	PROJECT NO: 141046



GENERAL NOTES — APPLIES TO ALL SHEETS

- THIS PROPERTY IS LOCATED IN THE HC ZONE AS DEPICTED ON THE MONTGOMERY TOWNSHIP TAX MAP SHEET 55.
- BOUNDARY AND EXISTING CONDITIONS FROM SURVEY ENTITLED, "LOCATION AND TOPOGRAPHIC SURVEY OF 1276 ROUTE 206, LOT 64 IN BLOCK 28001" DATED 4/21/2006, PREPARED BY BERTIN ENGINEERING ASSOCIATES, INC., GLEN ROCK, NJ.
- PROPOSED BUILDING FOOTPRINT FROM ARCHITECTURAL PLANS ENTITLED PROPOSED DUNKIN', DATED OCTOBER 20, 2020, PREPARED BY GK+A ARCHITECTS, P.C., RUTHERFORD, NJ.
- EXISTING SANITARY SEWER INFORMATION TAKEN FROM A PLAN ENTITLED "SANITARY SEWER EXTENSION BLOCK 29002, LOT 46 & ROUTE 206 CONSTRUCTION PLAN & DETAILS" DATED SEPTEMBER 16, 2005, LAST REVISED SEPTEMBER 24, 2009 PREPARED BY MENLO ENGINEERING ASSOCIATES, INC., HIGHLAND PARK, NJ.
- SOIL BOUNDARIES AND IDENTIFICATIONS DEPICTED ON THE PLANS TAKEN FROM THE USDA WEB SOIL SURVEY. THIS PROJECT PROPOSES <1 ACRE OF DISTURBANCE AND <0.25 ACRES OF NEW IMPERVIOUS SURFACES, AND IS NOT CONSIDERED AN NDEP "MAJOR DEVELOPMENT".
- ALL CONSTRUCTION ON THIS PROJECT SHALL BE IN ACCORDANCE WITH TOWN STANDARDS AND INSPECTIONS AND AS GOVERNED BY THE 2007 U.S. CUSTOMARY ENGLISH STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, OF THE NEW JERSEY DEPARTMENT OF TRANSPORTATION.
- CONTRACTOR TO NOTIFY TOWNSHIP ENGINEER 48 HOURS IN ADVANCE OF ANY LAND DISTURBANCE.
- THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL. THESE PLANS SHALL NOT BE CONSIDERED CONSTRUCTION DOCUMENTS UNTIL ALL APPLICABLE PERMITS HAVE BEEN OBTAINED BY THE DEVELOPER. ALL CONDITIONS OF THE MUNICIPAL RESOLUTION HAVE BEEN SATISFIED, AND THE PLANS HAVE BEEN SIGNED BY THE BOARD CHAIR, BOARD SECRETARY AND BOARD ENGINEER.
- THESE PLANS ARE NOT VALID UNLESS SIGNED AND SEALED BY THE LICENSED PROFESSIONAL SHOWN HEREON.
- ALL PROPOSED UTILITIES SHALL BE INSTALLED UNDERGROUND. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE EXISTING UTILITIES AND COORDINATING THE CONNECTION WITH MUNICIPALITY AND UTILITY COMPANIES.
- THERE SHALL BE NO BURYING OF DEBRIS, TREES, STUMPS OR CONSTRUCTION MATERIAL ONSITE.
- THE PROPOSED STRUCTURE IS TO BE SERVED BY PUBLIC WATER & SEWER.
- THERE ARE NO FRESHWATER WETLANDS AND WATERS PRESENT ON THE SUBJECT PROPERTY PER NJDEP L.O.I. FILE NO.1813-13-000211 ACTIVITY NUMBER FWH130003.
- THIS PROJECT IS NOT SUBJECT TO ANY FLOOD HAZARD AREAS OR SPECIAL WATER RESOURCE PROTECTION AREAS.
- THERE ARE NO CATEGORY ONE STREAMS AFFECTED BY THE PROPOSED CONSTRUCTION PER NJDEP GEO WEB MAPPING SOFTWARE.
- OWNER/APPLICANT/BUILDER/CONTRACTOR TO NOTIFY DESIGN ENGINEER IMMEDIATELY IF ANY PLAN INCONSISTENCIES EXIST.
- NO TOPSOIL SHALL BE REMOVED FROM THE SITE OR USED AS SPILL.
- OUTSIDE AGENCY APPROVALS ARE REQUIRED FROM THE SOMERSET COUNTY SOIL CONSERVATION DISTRICT AND THE SOMERSET COUNTY PLANNING BOARD.
- CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY CODES. APPLICABLE SAFETY CODES SHALL MEAN THE LATEST EDITION INCLUDING ANY AND ALL AMENDMENTS, REVISIONS AND ADDITIONS THERETO OF THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION'S "OCCUPATIONAL SAFETY AND HEALTH STANDARDS" (OSHA); SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION OF THE STATE OF NEW JERSEY, DEPARTMENT OF LABOR AND INDUSTRY, BUREAU OF ENGINEERING AND SAFETY; "CONSTRUCTION SAFETY CODE" AND "MAINTENANCE, CONSTRUCTION AND DEMOLITION," AND "BUILDING CODE".
- ALL SOIL TESTING FOR STRUCTURAL COMPONENTS TO BE DONE BY OTHERS. THIS INCLUDES BUT IS NOT LIMITED TO STRUCTURAL STABILITY, WATER TABLE ELEVATION, PRESENCE OR ABSENCE OF BURIED OR UNSUITABLE MATERIAL AND DEPTH TO ROCK. SITE DESIGN ENGINEER ACCEPTS NO LIABILITY FOR LOSS TIME OR ADDITIONAL EXPENSE DUE TO CHANGES OCCURRING FROM AFORESAID CONDITIONS.
- BUILDER/OWNER/CONTRACTOR OR AUTHORIZED REPRESENTATIVE TO APPROVE PROPOSED BUILDING ELEVATIONS AND LOCATION PRIOR TO ANY EXCAVATION. IF CHANGES ARE REQUESTED, THE SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOSS TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM FIELD MODIFICATIONS.
- THE OWNER, BY AUTHORIZING FERRIERO ENGINEERING, INC. TO PERFORM CONSTRUCTION STAKE OUT OF ANY COMPONENT OF THE WORK HAS ACKNOWLEDGED THEIR REVIEW AND APPROVAL OF THE LOCATION AND ELEVATION OF ALL IMPROVEMENTS SHOWN.
- THE STANDARD OF CARE FOR ALL PROFESSIONAL ENGINEERING AND RELATED SERVICES PERFORMED OR FURNISHED BY FEI UNDER THIS AGREEMENT WILL BE THE CARE AND SKILL ORDINARILY USED BY MEMBERS OF THE SUBJECT PROFESSION PRACTICING UNDER SIMILAR CIRCUMSTANCES AT THE SAME TIME AND IN THE SAME LOCALITY. FEI MAKES NO WARRANTIES, EXPRESSED OR IMPLIED, UNDER THIS AGREEMENT OR OTHERWISE, IN CONNECTION WITH THE SERVICES PROVIDED.
- FERRIERO ENGINEERING, INC. MAY USE ALL INFORMATION CONTAINED IN OUTSIDE CONSULTANTS REPORTS, DATA, SURVEYS AND INSTRUCTIONS FROM ANY CONSULTANTS THAT HAVE BEEN HIRED BY THE CLIENT TO WORK ON THIS PROJECT IN PERFORMING ITS SERVICES AND IS ENTITLED TO RELY UPON THE ACCURACY AND COMPLETENESS THEREOF. FEI SHALL NOT BE HELD RESPONSIBLE FOR ANY ERRORS OR OMISSIONS THAT MAY ARISE AS A RESULT OF ERRONEOUS OR INCOMPLETE INFORMATION PROVIDED BY THE CLIENT AND/OR THE CLIENT'S CONSULTANTS AND/OR CONTRACTORS.
- FEI SHALL NOT AT ANY TIME SUPERVISE, DIRECT, OR HAVE CONTROL OVER ANY CONTRACTORS WORK. NOR SHALL FEI HAVE AUTHORITY OVER OR RESPONSIBILITY FOR THE MEANS, METHODS, TECHNIQUES, SEQUENCES, OR PROCEDURES OF CONSTRUCTION SELECTED OR USED BY ANY CONTRACTOR, FOR SAFETY PRECAUTIONS AND PROGRAMS OF INCIDENT TO A CONTRACTORS WORK PROGRESS, NOR FOR ANY FAILURE OF ANY CONTRACTOR TO COMPLY WITH LAWS AND REGULATIONS APPLICABLE TO CONTRACTORS WORK. THE CLIENT WAIVES ANY CLAIMS AGAINST FERRIERO ENGINEERING, INC. ARISING FROM ERRORS, OMISSIONS, OR INACCURACIES IN DOCUMENTS OR OTHER INFORMATION PROVIDED TO US BY OTHERS.
- FEI SHALL NOT BE RESPONSIBLE FOR THE ACTS OR OMISSIONS OF ANY CONTRACTOR, SUBCONTRACTOR, SUPPLIER, OR OF ANY CONTRACTORS AGENTS OR EMPLOYEES OR ANY OTHER PERSONS AT THE PROJECT SITE OR OTHERWISE FURNISHING OR PERFORMING ANY CONSTRUCTION WORK, OR FOR ANY DECISION MADE ON INTERPRETATIONS OR CLARIFICATIONS OF THE CONSTRUCTION CONTRACT GIVEN BY CLIENT WITHOUT THE CONSULTATION AND ADVICE OF ENGINEER.
- ALL DESIGN DOCUMENTS PREPARED OR FURNISHED BY FEI ARE INSTRUMENTS OF SERVICE, AND FEI RETAINS AN OWNERSHIP AND PROPERTY INTEREST (INCLUDING COPYRIGHT AND THE RIGHT TO REUSE) IN SUCH DOCUMENTS, WHETHER OR NOT THE PROJECT IS COMPLETED. THE CLIENT MAY OBTAIN COPIES FOR INFORMATION AND REFERENCE IN CONNECTION WITH THE PROJECT PROVIDED THAT FEES FOR SERVICES RENDERED HAVE BEEN PAID IN FULL. IN ADDITION, THE REASONABLE COSTS OF RESEARCH AND REPRODUCTION FOR COPIES OR RECORDS SHALL BE PAID.
- ALL DIMENSIONS SHOWN ON THE PLANS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER IF ANY DISCREPANCIES EXIST PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN CHANGES. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS IF SUCH NOTIFICATION HAS NOT BEEN GIVEN.
- SOLID WASTE TO BE DISPOSED OF BY CONTRACTOR IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL REGULATIONS.
- THE EXISTING UNDERGROUND UTILITY INFORMATION SHOWN ON THESE PLANS IS APPROXIMATE AND IS NOT GUARANTEED AS TO ACCURACY OR COMPLETENESS. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL INFORMATION. THE CONTRACTOR SHALL CALL UNDERGROUND UTILITIES 1 (800) 272-1000 FOR MARKOUT PRIOR TO EXCAVATION. TEST PITS SHALL BE EXCAVATED BY THE CONTRACTOR AT ALL CROSSINGS PRIOR TO CONSTRUCTION TO ASCERTAIN EXISTING LOCATIONS, ELEVATIONS, MATERIALS, AND SIZES. TEST PIT DATA SHALL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION TO PERMIT ADJUSTMENTS AS REQUIRED.
- ALL EXCAVATED UNSUITABLE MATERIAL MUST BE TRANSPORTED TO AN APPROVED DISPOSAL LOCATION.
- CONTRACTOR IS RESPONSIBLE FOR ALL SHORING REQUIRED DURING EXCAVATION AND SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT OSHA STANDARDS, AS WELL AS ADDITIONAL PROVISIONS TO ASSURE STABILITY OF CONTIGUOUS STRUCTURES, AS FIELD CONDITIONS DICTATE.
- STRUCTURES AND IMPROVEMENTS ON ADJACENT PROPERTIES ARE APPROXIMATE PER AERIAL PHOTOGRAPHY.
- THE CONTRACTOR IS RESPONSIBLE FOR VERIFICATION OF EXISTING TOPOGRAPHIC INFORMATION AND UTILITY INVERT ELEVATIONS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. CONTRACTOR TO ENSURE 0.75% MINIMUM SLOPE AGAINST ALL ISLANDS, GUTTERS, AND CURBS; 1.0% ON ALL CONCRETE SURFACES, AND 1.5% MINIMUM ON ASPHALT, TO PREVENT PONDING. ANY DISCREPANCIES THAT MAY AFFECT THE PUBLIC SAFETY OR PROJECT COST MUST BE IDENTIFIED TO THE ENGINEER IN WRITING IMMEDIATELY. PROCEEDING WITH CONSTRUCTION WITHOUT NOTIFICATION IS DONE AT THE CONTRACTORS OWN RISK.
- PROPOSED TOP OF CURB ELEVATIONS ARE GENERALLY 6" ABOVE EXISTING LOCAL ASPHALT GRADE UNLESS OTHERWISE NOTED. FIELD ADJUST TO CREATE A MINIMUM OF 0.75% GUTTER GRADE ALONG CURB SURFACE. ENGINEER TO APPROVE FINAL CURBING CUT SHEETS PRIOR TO INSTALLATION.
- SANITARY LATERALS SHALL BE PVC SCHEDULE 40 OR PVC SDR 35 UNLESS INDICATED OTHERWISE.
- ADJACENT IMPROVEMENTS TAKEN FROM AERIAL PHOTOGRAPHY.
- AN AS-BUILT PLAN CERTIFIED BY A PROFESSIONAL LAND SURVEYOR SHALL BE SUPPLIED AT THE COMPLETION OF CONSTRUCTION.
- A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.
- THIS PROJECT WILL REQUIRE 820 CY± OF FILL. CONTRACTOR TO VERIFY. PRIOR TO IMPORTING ANY MATERIAL, APPROVAL FROM MONTGOMERY TOWNSHIP PLANNING BOARD IS REQUIRED.
- MAXIMUM NUMBER OF 4 EMPLOYEES FOR THE MORNING SHIFT, 3 EMPLOYEES FOR THE AFTERNOON SHIFT AND 2 EMPLOYEES FOR THE EVENING SHIFT.
- HOURS OF OPERATION ARE FROM 5AM TO 10 PM 7 DAYS A WEEK.

STANDARD CONSTRUCTION NOTES FOR ROAD IMPROVEMENTS

- Prior to the initiation of any excavation or construction within any street, road, or right of way under the jurisdiction of the Somerset County Board of Chosen Freeholders, a Somerset County Road Opening Permit shall be obtained from the Office of the County Engineer.
- The Office of the County Engineer is to be notified seventy-two (72) hours in advance of commencement of construction of any improvements under the jurisdiction of the County of Somerset. Grade construction sheets will be submitted at this time. These grade construction sheets are to be signed and sealed by a professional land surveyor.
- As indicated in the "Manual on Uniform Traffic Control Devices," proper and sufficient construction warning signs are to be provided and maintained by contractors performing construction work along County roads. Said signs are to be maintained until construction is completed and approved by the appropriate County inspection personnel.
- All improvements under the jurisdiction of the County of Somerset are to be constructed in accordance with Somerset County Specifications.
- All mailboxes, located within the construction improvements in the County R.O.W. will be reset in accordance with the owner of the mailbox and the postmaster.
- Information monuments, markers, disks, rivets of the National Geodetic Control Survey may be obtained from The New Jersey Department of Transportation, Geodetic Division, 1035 Parkway Avenue, Trenton, NJ, telephone #(609) 530-5641; or the Somerset County Engineering Division, telephone #(908) 231-7024, ext. 7512.
- The New Jersey Geodetic Control Survey, at the above address, is to be notified two (2) weeks in advance of commencement of construction of any improvements in order to preserve the resetting of existing monuments, or installation of new monuments if required by the County.

OFF-STREET PARKING SCHEDULE

BASED ON SECTION 16-4.12g  
RESTAURANTS:

ONE (1) PARKING SPACE FOR EVERY THREE (3) SEATS  
14 SEATS PROPOSED  
14 SEATS/3 = 4.67 SPACES

TOTAL SPACES REQUIRED = 5.0 SPACES  
TOTAL SPACES PROVIDED = 16.0 SPACES

(1) ONE VAN ACCESSIBLE PARKING SPACE PROVIDED

OFF-STREET LOADING SCHEDULE

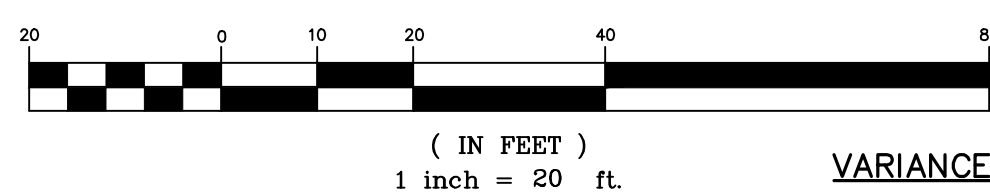
BASED ON SECTION 16.4.12h

ONE LOADING SPACE REQUIRED (15' X 40')

NO DEDICATED LOADING SPACE PROVIDED  
LOADING TO BE PERFORMED DURING OFF PEAK HOURS  
USING DRIVE AISLES (V)

(V) VARIANCE REQUIRED

SITE PLAN  
GRAPHIC SCALE



VARIANCES/DESIGN WAIVERS APPROVED SEPT. 22, 2015:

- 16-6.1m(1): D-3 VARIANCE REQUESTED FOR DRIVE THROUGH WINDOW FOR RESTAURANT. DUNKIN' INTENDS TO HAVE A DRIVE THROUGH WINDOW NOT LOCATED WITHIN A SHOPPING CENTER.
- 16-6.1m(2): D-3 VARIANCE REQUESTED FOR A DRIVE THROUGH WINDOW FOR DUNKIN' RESTAURANT WITH DIRECT VEHICULAR ACCESS TO A PUBLIC STREET.
- 16-4.12.d: MINIMUM LOT AREA REQUIREMENT IS 1 ACRE WHERE THE PROPOSED LOT AREA IS 0.5107 AC.
- 16-4.12.d: MINIMUM LOT WIDTH AND FRONTAGE REQUIREMENT IS 150 FEET WHERE THE PROPOSED IS 143.46 FEET.
- 16-4.12.d: MINIMUM LOT DEPTH REQUIREMENT IS 150 FT WHERE THE PROPOSED IS 147.58 FT.
- 16-4.12.d: MINIMUM REAR YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 33.5 FT.
- 16-4.12.d: LOT COVERAGE, THE MAXIMUM ALLOWABLE IS 55% WHERE 78.5% IS PROPOSED.
- 16-4.12e.5.a VISUAL BREAKS, THE BUILDING WILL HAVE BREAKS IN COLOR AND FINISHES.
- 16-4.12e.5.d FRONT FACADE SHALL BE CONTINUED AROUND ALL VISIBLY EXPOSED SIDES. STONE FINISH WAS ADDED TO ALL SIDES OF BUILDING.
- 16-4.12e.6.c ROOF TREATMENT, DECORATIVE FENCING WAS ADDED TO FLAT ROOF.
- 16-4.12e.7.b WINDOWS TO OCCUPY 75% OF FIRST STORY FACADE, 42% PROPOSED.
- 11.1. 16-4.12.f.4.a & 16-4.12f(5): NO PARKING AREA SHALL BE PERMITTED WITHIN THE FIRST TWENTY-FIVE (25) FEET ADJACENT TO ANY STREET NOR WITHIN THE FIRST FIFTEEN (15) FEET ADJACENT TO ANY OTHER PROPERTY LINE, WHEREAS 3.0' IS PROPOSED ALONG COUNTY ROUTE 518 AND 9.1' ALONG STATE HIGHWAY ROUTE 206.
- 16.4.12.h: OFF STREET LOADING SPACE REQUIRED AT SIDE OR REAR OF BUILDING. NO LOADING SPACE PROVIDED.
- 16-4.12.i: (1) FREE STANDING SIGN PERMITTED, 1 SIGN PROPOSED AND 2 MENU BOARDS PROPOSED.
- 16-5.4.b: THE AVERAGE LIGHT INTENSITY SHALL BE NO MORE THAN 1.0 FC WHERE 2.1 FC IS PROVIDED.

REQUESTED VARIANCES/DESIGN WAIVERS

- 16-4.12.d: MINIMUM FRONT YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 45.3 FT.
- 16-4.12.d: MINIMUM REAR YARD REQUIREMENT IS 50 FT WHERE THE PROPOSED IS 41.2 FT.
- 16-4.12.d: LOT COVERAGE, THE MAXIMUM ALLOWABLE IS 55% WHERE 84.4% IS PROPOSED.
- 16-4.12e.5.c BUILDINGS WITH 2 FRONTAGES SHALL HAVE 2 FACADES, DRIVE-UP WINDOW PROPOSED ON 518 FRONTAGE
- 16-4.12.f.3 REQUIRES 45% OF LOT TO BE LANDSCAPED, 2,865SF OR 12.9% PROPOSED
- 16-4.12f.4.a & 16-4.12.f.5: NO PARKING AREA SHALL BE PERMITTED WITHIN THE FIRST TWENTY-FIVE (25) FEET ADJACENT TO ANY STREET NOR WITHIN THE FIRST FIFTEEN (15) FEET ADJACENT TO ANY OTHER PROPERTY LINE, WHEREAS 4.0' IS PROPOSED ALONG COUNTY ROUTE 518 AND 9.1' ALONG STATE HIGHWAY ROUTE 206.
- 16-4.12.f.5 REQUIRES A 15' SETBACK FROM THE PROPERTY LINE FOR ALL STRUCTURES. PROPOSED PARKING, DRIVEWAY AND RETAINING WALL ARE CLOSER TO PROPERTY LINE. FACE OF RETAINING WALL LOCATED ALONG WESTERLY PROPERTY LINE.
- 16-5.4.b: THE AVERAGE LIGHT INTENSITY SHALL BE NO MORE THAN 1.0 FC WHERE 1.1 FC IS PROVIDED.
- 16-5.6.d.15 STREET TREES REQ'D. AT 50' INTERVALS, 2 STREET TREES PROPOSED, A VARIANCE IS REQUIRED FOR THIS ITEM.
- 16-5.13.f.5 ON-SITE DIRECTIONAL SIGNS, PARKING SIGNS, WARNING SIGNS AND OTHER SIMILAR SIGNS SHALL BE PERMITTED, PROVIDED THEY DO NOT EXCEED TWO SQUARE FEET IN AREA AND DO NOT DISPLAY ANY TYPE OF ADVERTISING. DIRECTIONAL SIGNS PROPOSED 2 SF REQUIRED, GREATER THAN 2 SF PROPOSED, SIGN SIZES VARY.

ZONE TABLE

HC-HIGHWAY COMMERCIAL 16-4.12	REQUIRED	EXISTING	PROPOSED - PRE R.O.W. DEDICATION	PROPOSED - POST R.O.W. DEDICATION
MIN. LOT AREA	1 ACRE	0.5197 AC	0.5197 AC (1)	0.5107 AC (2)
MIN. LOT FRONTAGE	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN. LOT WIDTH	150 FT	150.16 FT	150.16 FT	143.46 FT (2)
MIN LOT DEPTH	150 FT	151.68 FT	151.68 FT	151.68 FT
MIN FRONT YARD (3)	50 FT	61.55 FT	47.2 FT	45.3 FT (2)
MIN SIDE YARD (3)	25 FT	38.50 FT	44.7 FT	42.8 FT
MIN REAR YARD (3)	50 FT	49.3 FT	41.3 FT (2)	41.2 FT (2)
MAX. % IMPERVIOUS LOT COVERAGE	55 %	73.1% (1) 16,554 SF	82.9% (2) 18,767 SF	84.4% (2) 18,767 SF
MAX. BUILDING HEIGHT	30 FT	< 30 FT	28.00 FT (4)	28.00 FT (4)
MAX. FLOOR AREA RATIO	0.20	0.063	0.081 1823 SF	0.082 1823 SF

- (1) EXISTING NON-CONFORMING
- (2) VARIANCE REQUIRED
- (3) MEASURED TO CANOPY, IF APPLICABLE
- (4) MEASURED TO TOP OF CUPOLA

PROPOSED IMPERVIOUS AREA CALCULATIONS	
IMPROVEMENTS	AREAS
PROPOSED BUILDING	1,823 SF
PROPOSED PAVEMENT	12,701 SF
PROPOSED CONCRETE/WALKS	4,243 SF
TOTAL PROPOSED IMPERVIOUS	18,767 SF (84.4%)

THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL AND MAY NOT BE USED FOR CONSTRUCTION UNTIL ALL PERMITS HAVE BEEN OBTAINED

LEGEND

- 540 EXISTING CONTOUR LINE
- 520 PROPOSED CONTOUR LINE
- 516.3 EXISTING SPOT GRADE
- + 640.00 PROPOSED SPOT GRADE
- EXISTING CURB LINE
- PROPOSED CURB LINE
- EXISTING INLET
- PROPOSED INLET
- EXISTING STORM/SAN PIPE
- EXISTING STORM SEWER
- EXISTING SITE LIGHT
- PROPOSED SITE LIGHT
- EXISTING GAS LINE
- EXISTING WATER LINE
- EXISTING OVERHEAD WIRE
- EXISTING UTILITY POLE
- EXISTING STONE WALL
- EXISTING FENCE
- PROPOSED CONCRETE
- EXISTING SITE LIGHT
- DC DEPRESSED CURB
- FC FLUSH CURB WITH PAVEMENT
- GF GUTTER FLOW

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978

Ferriero  
Engineering, Inc.

180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24CA27935400

SITE PLAN

LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

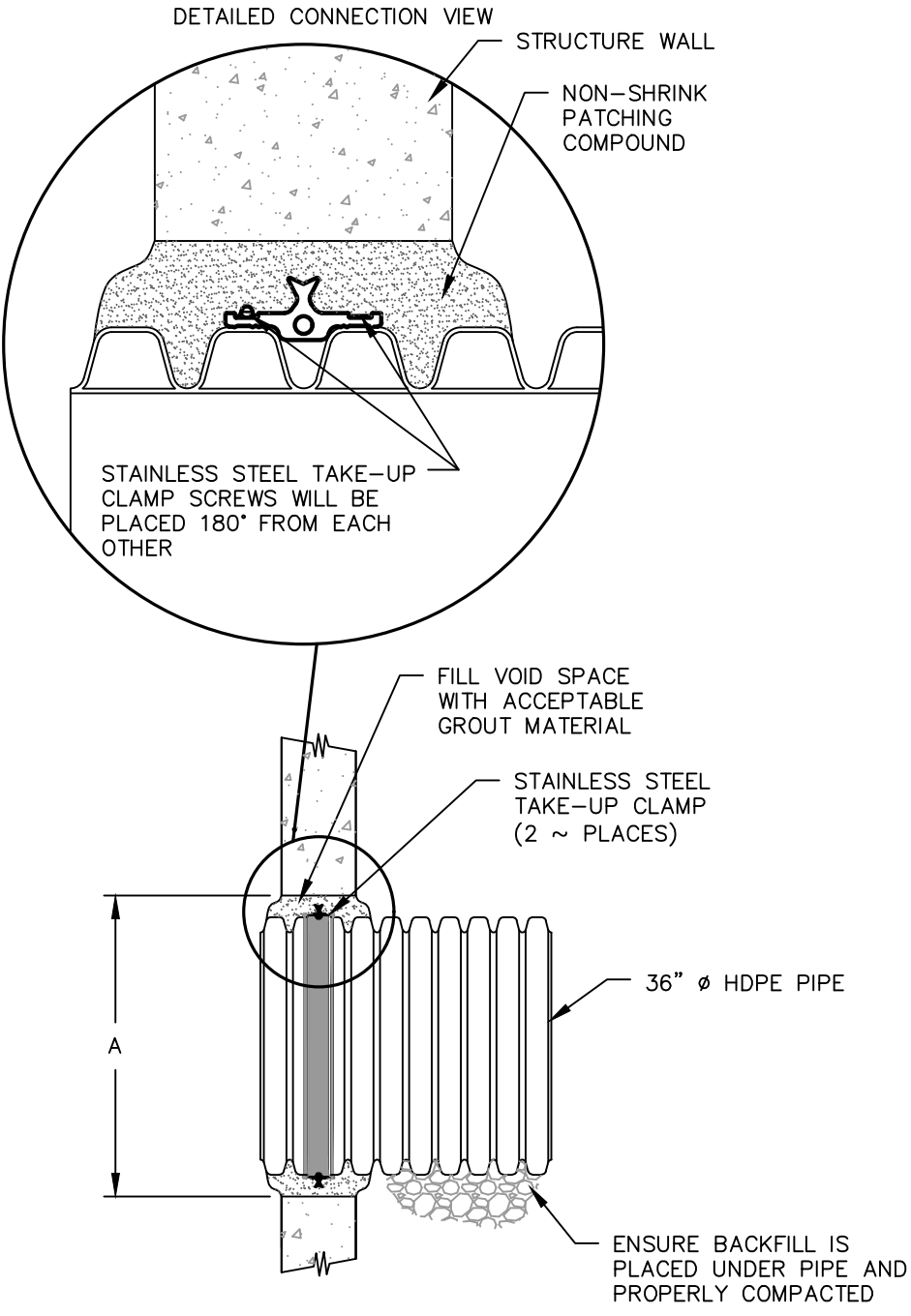
SHEET  
3 OF 13

DATE: 2/25/2021 PROJECT NO: 141046  
REVISION:



IMPORTANT NOTES:

- CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY CODES. APPLICABLE SAFETY CODES SHALL MEAN THE LATEST EDITION INCLUDING ANY AND ALL AMENDMENTS, REVISIONS AND ADDITIONS THERETO OF THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION'S "OCCUPATIONAL SAFETY AND HEALTH STANDARDS" (OSHA); "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION" OF THE STATE OF NEW JERSEY, DEPARTMENT OF LABOR AND INDUSTRY, BUREAU OF ENGINEERING AND SAFETY; "CONSTRUCTION SAFETY CODE," AND "MAINTENANCE, CONSTRUCTION AND DEMOLITION," AND "BUILDING CODE".
- ALL SOIL TESTING FOR STRUCTURAL COMPONENTS TO BE DONE BY OTHERS, THIS INCLUDES BUT IS NOT LIMITED TO STRUCTURAL STABILITY, WATER TABLE ELEVATION, FOOTING DESIGN, PRESENCE OR ABSENCE OF BURIED OR UNSATURATED MATERIAL AND DEPTH TO ROCK. SITE DESIGN ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO CHANGES OCCURRING FROM AFORESAID CONDITIONS.
- BUILDER/OWNER/CONTRACTOR OR AUTHORIZED REPRESENTATIVE TO APPROVE PROPOSED BUILDING ELEVATIONS AND LOCATION PRIOR TO ANY EXCAVATION. IF CHANGES ARE REQUESTED, THE SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM FIELD MODIFICATIONS.
- THE OWNER, BY AUTHORIZING FERRIERO ENGINEERING, INC. TO PERFORM CONSTRUCTION STAKE OUT OF ANY COMPONENT OF THE WORK HAS ACKNOWLEDGED THEIR REVIEW AND APPROVAL OF THE LOCATION AND ELEVATION OF ALL IMPROVEMENTS SHOWN.
- THESE PLANS ARE NOT VALID UNLESS SIGNED AND SEALED BY THE LICENSED PROFESSIONAL SHOWN HEREON.
- OWNER/APPLICANT/BUILDER/CONTRACTOR TO NOTIFY DESIGN ENGINEER IF ANY PLAN INCONSISTENCIES EXIST.
- CUT SHEETS FOR CURBING SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR REVIEW PRIOR TO INSTALLING CURBING.
- AN AS-BUILT PLAN CERTIFIED BY A PROFESSIONAL LAND SURVEYOR SHALL BE SUPPLIED AT THE COMPLETION OF CONSTRUCTION.
- A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.

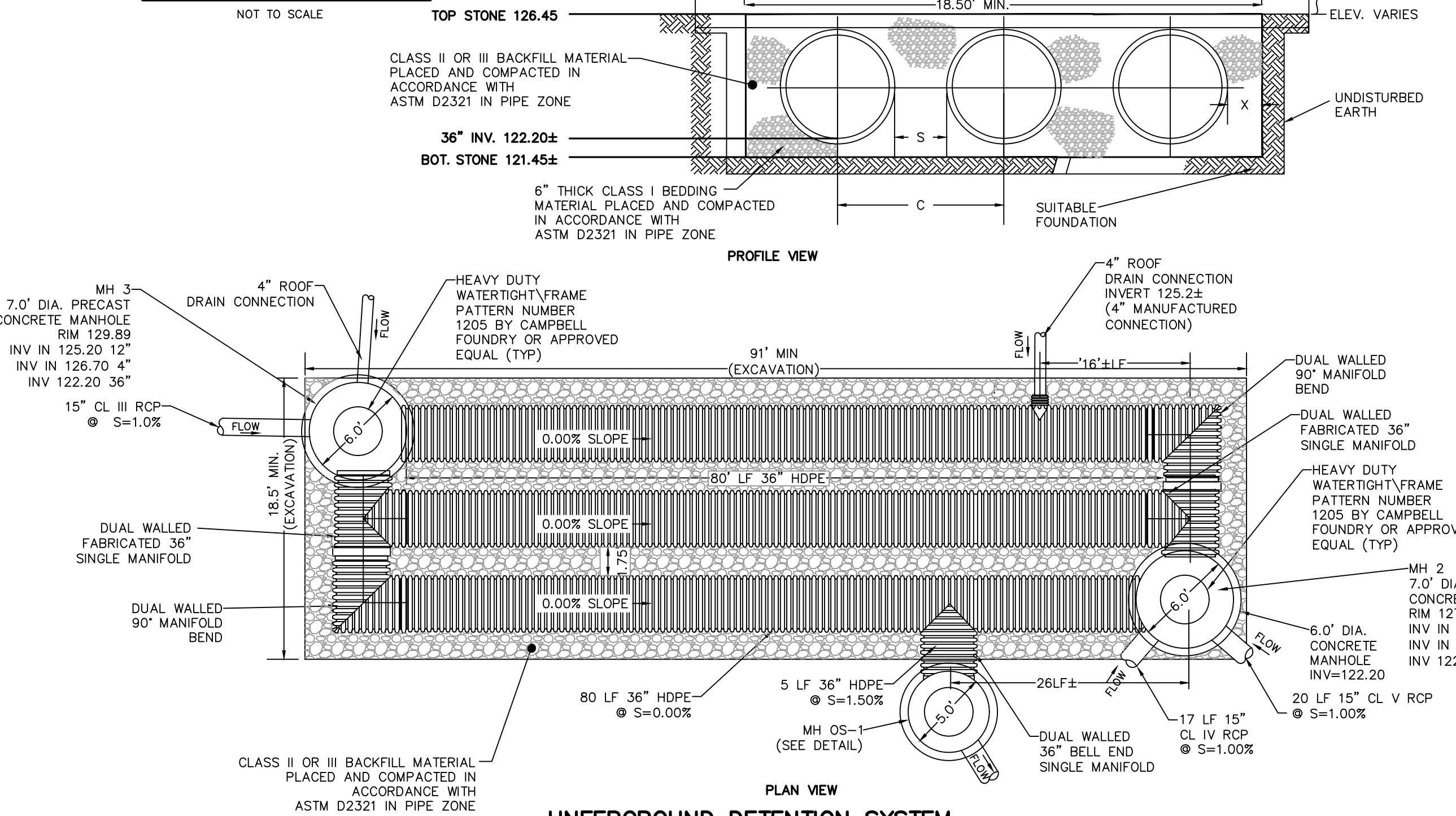


PIPE OD		A-PROFILE		H-PROFILE		MIN. DISTANCE PIPE INVERT TO STRUCTURE INVERT		ADS PRODUCT CODES	
PIPE SIZE		41"	41"	41"	41"	47.00"	5.5"		
		(1043.9mm)	(1043.9mm)	(1043.9mm)	(1043.9mm)	(1193.7mm)	(139.7mm)		3602PS

NOTES:

- PERFORMANCE HIGHLY DEPENDENT ON INSTALLATION. CONTRACTOR MUST ENSURE MANHOLE GASKET IS UNIFORMLY SEATED AROUND STRUCTURE ADAPTER. EXTRA PRECAUTIONS MUST BE TAKEN TO PREVENT DIFFERENTIAL SETTLEMENT BETWEEN THE PIPE AND MANHOLE.

36" HDPE WATERSTOP GROUDED STRUCTURE CONNECTION

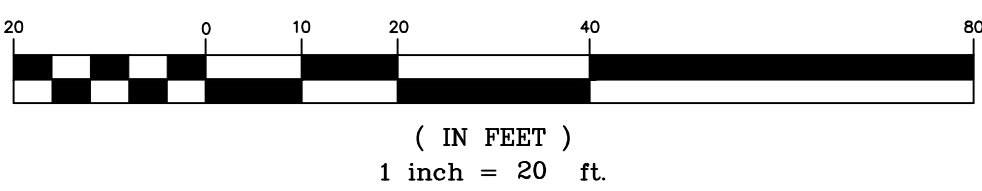


UNDERGROUND DETENTION SYSTEM

NOT TO SCALE

GRADING, DRAINAGE & UTILITY PLAN

GRAPHIC SCALE



NOMINAL DIAMETER	NOMINAL O.D.	TYPICAL SPACING "S" (900 MM)	TYPICAL SPACING "C" (530 MM)	TYPICAL SIDE WALL "X" (1600 MM)	H (460 MM)
36"	42"	21"	63"	18"	12"
(900 MM)	(1067 MM)	(530 MM)	(1600 MM)	(460 MM)	(292 MM)

NOTES:

- ALL PIPING FOR THE RETENTION SYSTEM SHALL BE SOLID 36-INCH DIAMETER DUAL WALLED CORRUGATED N-12 POLYETHYLENE WATER TIGHT PIPE MANUFACTURED BY ADS OR EQUIV.
- DETENTION SYSTEM OPEN EXCAVATION DIMENSIONS: (18'-6" x 91'-0" MIN.)

PROPOSED SANITARY SEWER FLOW PER DAY (NJAC 7:14A-23.3)

FAST FOOD RESTAURANT: 15 GAL/DAY/SEAT  
14 SEATS  
210 GAL/DAY

SANITARY SEWER DISCHARGES TO STAGE II SEWAGE TREATMENT PLANT NJ0026905

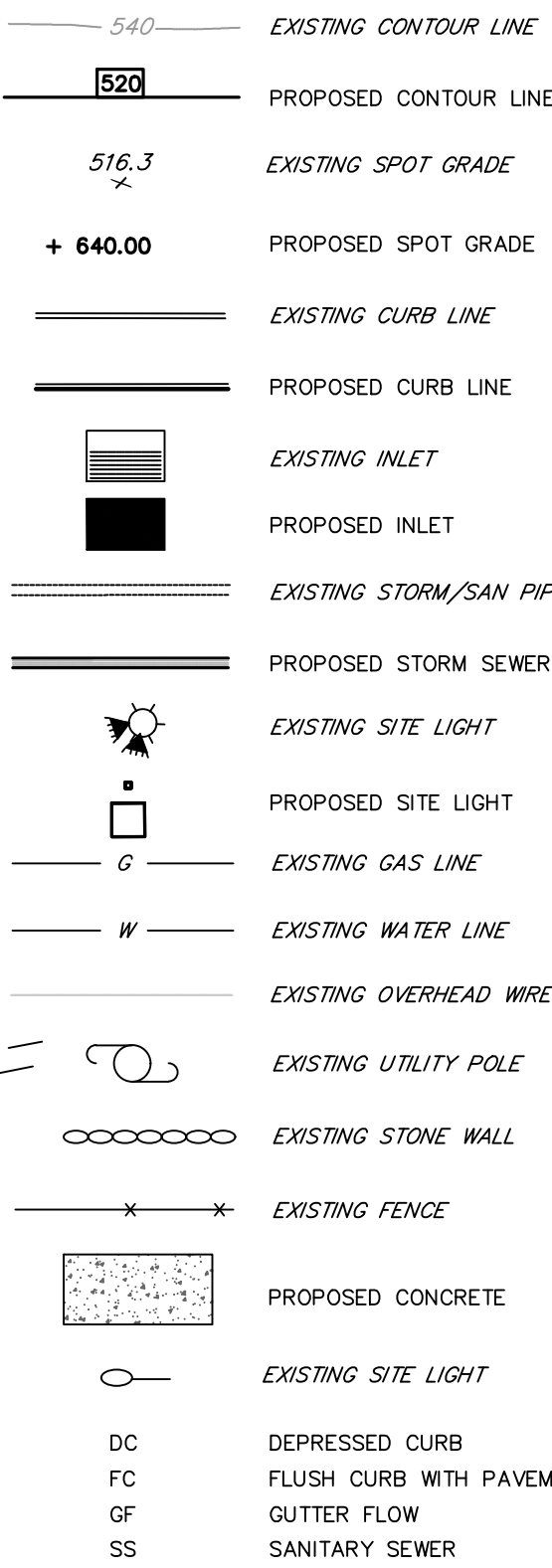
Cut/Fill Report

Generated: 2023-02-11 08:22:40  
By user: ALHanson  
Drawing: Z:\141046.DWG\BASEMAPS\Montgomery Twp\Z:\141046.DWG\BASEMAPS\Montgomery Twp\141046-BASEMAP.DWG

Volume Summary		Cut		Fill		Net	
Name	Type	Cut Factor	Fill Factor	2d Area (Sq. Ft.)	Cut (Cu. Yd.)	Fill (Cu. Yd.)	Net (Cu. Yd.)
Volume	fill	1.000	1.000	20794.64	42.20	861.71	\$19.51-Fill>
Totals							
Total				20794.64	42.20	861.71	\$19.51-Fill>

\* Value adjusted by cut or fill factor other than 1.0

LEGEND



BUILDING HEIGHT:

MEAN SPOT GRADE ELEVATION ALONG BUILDING  
130.60+131.50+131.50+130.94+130.06+130.45+130.75=915.80/7=130.83

PER 16-2.1 DEFINITIONS - BUILDING HEIGHT SHALL MEAN THE VERTICAL DISTANCE MEASURED TO A BUILDING FROM THE MEAN ELEVATION OF THE FINISHED GRADES ALONG ALL SIDES OF THE BUILDING; PROVIDED THAT IF THE FINISHED GRADE IS HIGHER THAN THE PREDEVELOPMENT GRADE AT ANY POINT BENEATH THE BUILDING, THEN THE BUILDING HEIGHT SHALL BE MEASURED FROM AN ELEVATION NO HIGHER THAN ONE FOOT ABOVE THE HIGHEST POINT OF THE PREDEVELOPMENT GRADE BENEATH THE BUILDING.

\*EXISTING GRADE AT EXISTING BUILDING = 129.1, 129.1 + 1.0 = 130.1 MEAN ELEVATION

MAXIMUM BUILDING HEIGHT ELEVATION AT TOP OF ROOF = 152.93  
MAXIMUM BUILDING HEIGHT ELEVATION AT TOP OF CUPOLA = 158.10

MAXIMUM BUILDING HEIGHT (TOP OF ROOF) = (152.93-130.10) = 22.83'  
MAXIMUM BUILDING HEIGHT (TOP OF CUPOLA) = (158.10-130.10) = 28.00'

\*AVERAGE SPOT GRADE ELEVATION DETERMINED USING THE HIGHEST POINT OF THE PREDEVELOPMENT GRADE BENEATH THE EXISTING BUILDING PLUS ONE FOOT.

18LF 15" CL IV RCP PIPE @ S=1.0%  
INV OUT 124.40 15"  
PROPOSED CB-1  
GRIT 128.00  
INV OUT 124.40 15"  
21LF 15" CL V RCP PIPE @ S=1.0%  
PROPOSED CB-2  
GRIT 126.63  
INV OUT 124.43 15"

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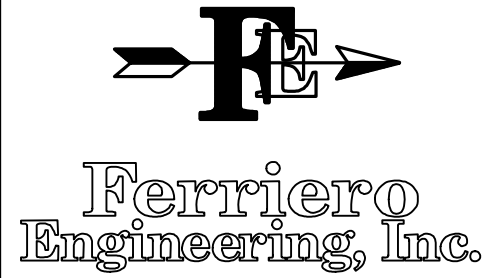
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PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978



180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24GA27935400

GRADING, DRAINAGE & UTILITY PLAN

DUNKIN'

LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET  
4 OF 13

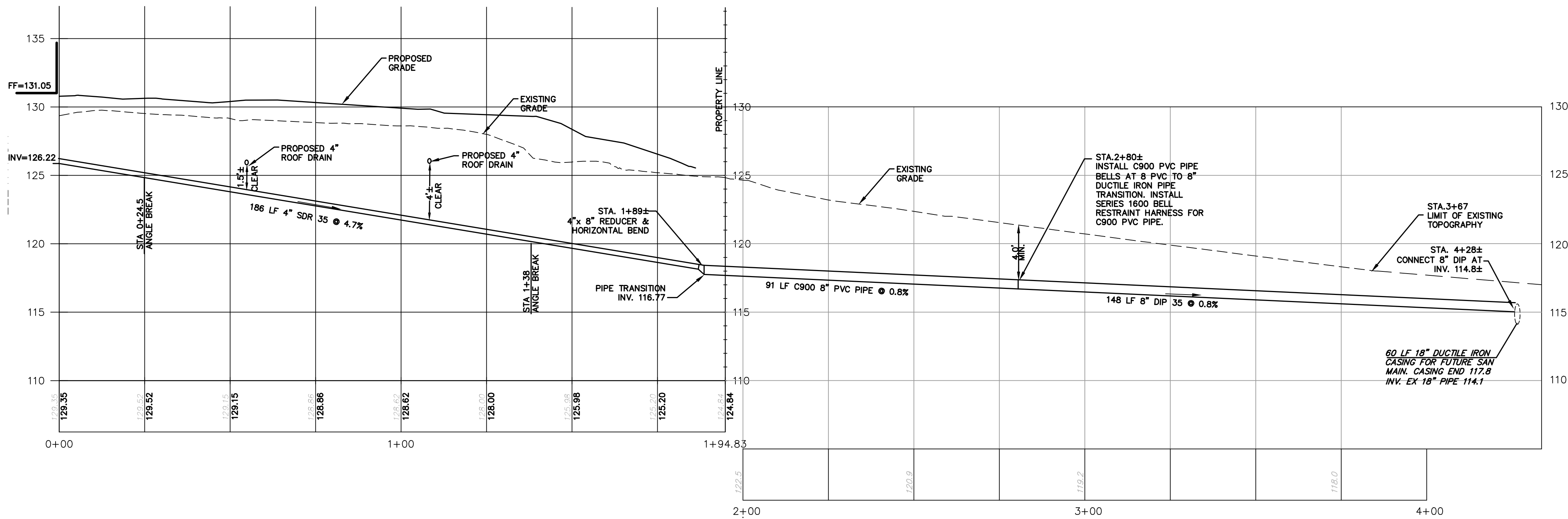
DATE: 2/25/2021 PROJECT NO:  
REVISION: 141046

STORMFILTER BY CONTECH

NOT TO SCALE BY CONTECH OR APPROVED EQUAL

THESE PLANS HAVE BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL AND MAY NOT BE USED FOR CONSTRUCTION UNTIL ALL PERMITS HAVE BEEN OBTAINED



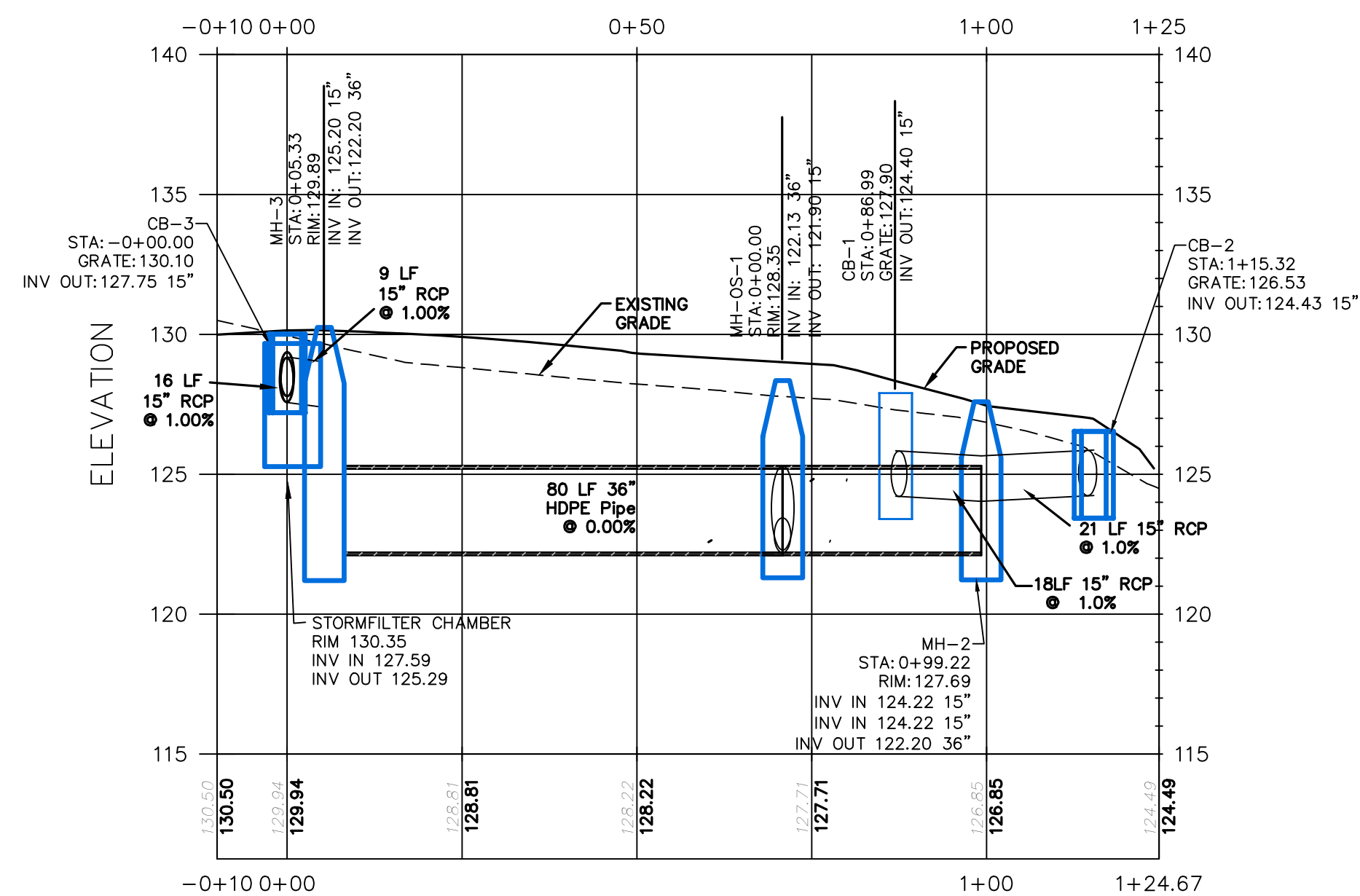


#### SANITARY SEWER PROFILE

SCALE: 1" = 20' HORIZ.  
1" = 5' VERT.

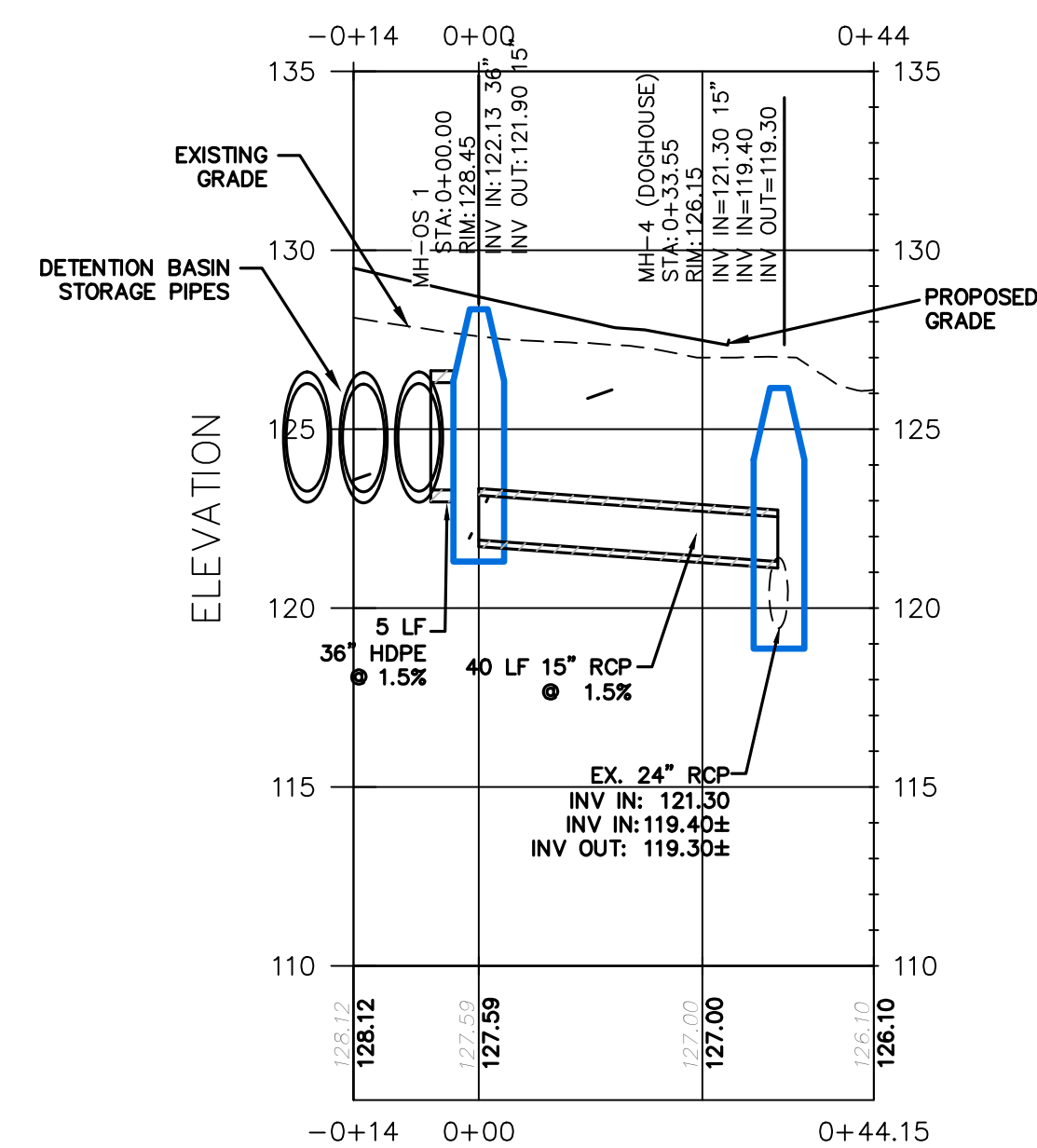
PROPOSED SANITARY SEWER FLOW PER DAY (NJAC 7:14A-23.3)  
FAST FOOD RESTAURANT: 15 GAL/DAY/SEAT  
14 SEATS  
210 GAL/DAY

SANITARY SEWER DISCHARGES TO STAGE II SEWAGE TREATMENT PLANT NJ0026905



#### DRAINAGE PIPES TO DETENTION BASIN

SCALE: 1" = 20' HORIZ.  
1" = 5' VERT.



#### DETENTION BASIN TO OUTFALL

SCALE: 1" = 20' HORIZ.  
1" = 5' VERT.

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NO.	DATE	REVISION
1	5/20/2021	PER TWP COMPLETENESS REVIEW
DRAWN BY: SCALE: FIELD BOOK:		
AMH AS NOTED		
CHKD BY: FILE:		
PWF #1046/DWG/BASEMAPS/BASEMAPS-9-2020		
PAUL W. FERRIERO N.J. PROFESSIONAL ENGINEER NO. GE32978		
180 MAIN STREET P.O. BOX 571 CHESTER, NEW JERSEY 07930 908-879-6209		
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SANITARY & STORM SEWER PROFILES		
DUNKIN!		
LOT 64 BLOCK 28005 MONTGOMERY TOWNSHIP SOMERSET COUNTY NEW JERSEY		
SHEET 5 OF 13		
DATE: 2/25/2021	PROJECT NO: 141046	



SOMERSET - UNION SOIL CONSERVATION DISTRICT SOIL EROSION AND SEDIMENT CONTROL NOTES

- ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSTALLED IN ACCORDANCE WITH THE NEW JERSEY STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL, AND WILL BE IN PLACE PRIOR TO ANY MAJOR SOIL DISTURBANCE, OR IN THEIR PROPER SEQUENCE AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
- ANY DISTURBED AREA THAT WILL BE LEFT EXPOSED FOR MORE THAN THIRTY (30) DAYS AND NOT SUBJECT TO CONSTRUCTION TRAFFIC SHALL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PROHIBITS TEMPORARY SEEDING, THE DISTURBED AREA WILL BE MULCHED WITH SALT HAY OR EQUIVALENT AND BOUND IN ACCORDANCE WITH THE NEW JERSEY STANDARDS. (i.e. PEG AND TWINE, MULCH NETTING, OR LIQUID MULCH BINDER).
- PERMANENT VEGETATION IS TO BE ESTABLISHED ON EXPOSED AREAS WITHIN TEN (10) DAYS AFTER FINAL GRADING. MULCH IS TO BE USED FOR PROTECTION UNTIL VEGETATION IS ESTABLISHED.
- IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING ALL CRITICAL AREAS SUBJECT TO EROSION WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR SUITABLE EQUAL, AT A 2 TON/ACRE RATE, ACCORDING TO THE STATE STANDARDS.
- TEMPORARY BERM ARE TO BE INSTALLED ON ALL CLEARED ROADWAYS AND EASEMENT AREAS IN ACCORDANCE WITH SECTION 4.21 OF THE STATE STANDARDS.
- PERMANENT SEEDING AND STABILIZATION TO BE IN ACCORDANCE WITH THE STANDARDS FOR PERMANENT VEGETATIVE COVER. SPECIFIED RATES AND LOCATIONS SHALL BE AS ON THE APPROVED SOIL EROSION AND SEDIMENT CONTROL PLAN.
- THE SITE SHALL AT ALL TIMES BE GRADED AND MAINTAINED SUCH THAT ALL STORM WATER RUNOFF IS DIVERTED TO SOIL EROSION AND SEDIMENT CONTROL FACILITIES.
- ALL SEDIMENTATION STRUCTURES WILL BE INSPECTED AND MAINTAINED REGULARLY.
- STOCKPILES SHALL NOT BE LOCATED WITHIN 50' OF A FLOOD PLAIN, SLOPE, DRAINAGE FACILITY, OR ROADWAY. ALL STOCKPILE BASES SHALL BE PROTECTED BY A HAY BALE BARRIER OR SEDIMENT FENCE.
- A CRUSHED STONE, VEHICLE WHEEL-CLEANING BLANKET WILL BE INSTALLED.
- ALL NEW ROADWAYS WILL BE TREATED WITH A SUITABLE SUBBASE UPON ESTABLISHMENT OF FINAL GRADE ELEVATIONS.
- PAVED ROADWAYS MUST BE KEPT CLEAN AT ALL TIMES.
- ALL CATCH BASIN INLETS WILL BE PROTECTED WITH A CRUSHED STONE AND FILTER FABRIC. FILTER DETAILS APPEAR ON PLAN.
- BEFORE DISCHARGE POINTS BECOME OPERATIONAL, ALL STORM DRAINAGE OUTLETS WILL BE STABILIZED AS REQUIRED.
- ALL DEWATERING OPERATIONS MUST DISCHARGE DIRECTLY INTO A SEDIMENT FILTER AREA. THE SEDIMENT FILTER SHOULD BE OF SUITABLE SEDIMENT FILTER FABRIC. SEE DETAILS.
- ALL SEDIMENTATION BASINS WILL BE CLEANED WHEN THE CAPACITY HAS BEEN REDUCED BY 50%. A CLEAN OUT ELEVATION WILL BE IDENTIFIED ON THE PLAN AND A MARKER INSTALLED ON THE SITE.
- DURING AND AFTER CONSTRUCTION, THE OWNER WILL BE RESPONSIBLE FOR THE MAINTENANCE AND UPKEEP OF THE DRAINAGE STRUCTURES, VEGETATIVE COVER, AND ANY OTHER MEASURES DEEMED APPROPRIATE BY THE SOIL CONSERVATION DISTRICT. SAID RESPONSIBILITY WILL PRECLUDE WHEN COMPLETED WORK IS APPROVED BY THE SOIL CONSERVATION DISTRICT.
- ALL TREES TO REMAIN AFTER CONSTRUCTION ARE TO BE PROTECTED WITH TREE PROTECTION DEVICES OR SEDIMENT BARRIERS. SEE STANDARDS.
- THE SOIL CONSERVATION DISTRICT MAY REQUEST ADDITIONAL MEASURES TO MINIMIZE ON OR OFF SITE EROSION PROBLEMS DURING CONSTRUCTION.

TOPSOIL STOCKPILE PROTECTION

- CONSTRUCT TEMPORARY DIVERSION BERM AND/OR HAY BALE BARRIER AROUND STOCKPILE AREA AS REQUIRED.
- APPLY LIMESTONE AT A RATE OF 90 lbs/1000 SF.
- APPLY FERTILIZER (10-10-10) AT A RATE OF 4 LBS/ACRE OR 11 lbs/1000 SF.
- APPLY PERENNIAL RYEGRASS AT A RATE OF 1 lb/1000 SF.
- MULCH WITH UNROTTED SALT HAY OR SMALL GRAIN STRAW IMMEDIATELY AFTER SEEDING. APPLY AT A RATE OF 90 lbs/1000 SF.

TEMPORARY STABILIZATION SPECIFICATIONS

- APPLY GROUND LIMESTONE AT A RATE OF 90 lbs/1000 SF.
- APPLY FERTILIZER (10-10-10) AT A RATE OF 14 lbs/1000 SF AND WORK INTO SOIL 4" DEEP.
- APPLY SEED MIXTURE: PERENNIAL RYEGRASS AT 40 lbs/ACRE AND ANNUAL RYEGRASS AT 40 lbs/ACRE OR APPROVED EQUAL.
- MULCH WITH UNROTTED SALT HAY OR SMALL GRAIN STRAW IMMEDIATELY AFTER SEEDING. APPLY AT A RATE OF 90 lbs/1000 SF AND SECURE BY APPROVED METHODS (i.e. PEG AND TWINE, MULCH NETTING, OR LIQUID MULCH BINDER).
- PLANT SEED BETWEEN MARCH 1 AND MAY 15 OR BETWEEN AUGUST 15 AND OCTOBER 1 IF POSSIBLE.

PERMANENT STABILIZATION SPECIFICATIONS

- ALL TOPSOIL STRIPPED FROM THE SITE TO BE STOCKPILED AND REAPPLIED TO THE SUBJECT SITE UPON COMPLETION OF GRADING. NO TOPSOIL TO BE REMOVED FROM THE LOT.
- TOPSOIL SHOULD BE HANDLED ONLY WHEN IT IS DRY ENOUGH TO BE WORKED WITH WITHOUT DAMAGING THE SOIL STRUCTURE. A UNIFORM APPLICATION TO A DEPTH OF 5 INCHES (UNSETTLED) IS REQUIRED ON ALL SITES.
- APPLY GROUND LIMESTONE AT A RATE OF 90 lbs/1000 SF.
- APPLY FERTILIZER (10-10-10) AT A RATE OF 14 lbs/1000 SF.
- APPLY SEED MIXTURE: PERENNIAL RYEGRASS AT 10 lbs/ACRE, KENTUCKY BLUEGRASS AT 25 lbs/ACRE, RED FESCUE AT 15 lbs/ACRE, AND SPREADING FESCUE AT 15 lbs/ACRE OR APPROVED EQUAL.
- MULCH WITH UNROTTED SALT HAY OR SMALL GRAIN STRAW IMMEDIATELY AFTER SEEDING. APPLY AT A RATE OF 90 lbs/1000 SF ACCORDING TO THE NEW JERSEY STANDARDS.
- ANCHOR MULCH WITH ORGANIC & VEGETABLE BASED OR SYNTHETIC BINDERS APPLIED AT RATES AND WEATHER CONDITIONS AS SPECIFIED BY THE MANUFACTURER. OTHER APPROVED METHODS (i.e. PEG AND TWINE, OR MULCH NETTING) MAY BE USED. IF POSSIBLE, PLANT BETWEEN MARCH 1 & MAY 15, OR BETWEEN AUGUST 15 & OCTOBER 1.

NOTE: 72 HOURS PRIOR TO ANY SOIL DISTURBANCE, NOTICE OF SUCH IN WRITING SHALL BE GIVEN TO THE SOIL CONSERVATION DISTRICT AND THE WARREN TOWNSHIP ENGINEER.

DUST CONTROL NOTES

THE FOLLOWING METHODS SHOULD BE CONSIDERED FOR CONTROLLING DUST:

MULCHES - SEE STANDARDS FOR STABILIZATION WITH MULCHES ONLY (PG. 5-1).

VEGETATIVE COVER - SEE STANDARDS FOR TEMPORARY VEGETATIVE COVER (PG. 7-1), PERMANENT VEGETATIVE COVER FOR SOIL STABILIZATION (PG. 4-1), AND PERMANENT STABILIZATION WITH SOD (PG. 6-1).

SPRAY ON ADHESIVES - ON MINERAL SOILS (NOT EFFECTIVE ON MUCK SOILS). KEEP TRAFFIC OFF THESE AREAS.

MATERIAL	WATER DILUTION	TYPE OF NOZZLE	APPLY GALLONS/ACRE
ANIONIC ASPHALT EMULSION	7:1	COARSE SPRAY	1,200
LATEX EMULSION	12.5:1	FINE SPRAY	235
RESIN IN WATER	4:1	FINE SPRAY	300
POLYACRYLAMIDE (PAM) - SPRAY ON	APPLY ACCORDING TO MANUFACTURER'S INSTRUCTIONS. MAY ALSO BE USED AS AN ADDITIVE TO SEDIMENT BASINS TO FLOCCULATE AND PRECIPITATE SUSPENDED COLLOIDS. SEE SEDIMENT BASIN STANDARD (PG. 26-1).		
POLYACRYLAMIDE (PAM) - DRY SPRAY			
ACIDULATION SOY BEAN SOAP STICK	NONE	COARSE SPRAY	1200

TILLAGE - TO ROUGHEN SURFACE AND BRING CLODS TO THE SURFACE. THIS IS A TEMPORARY EMERGENCY MEASURE WHICH SHOULD BE USED BEFORE SOIL BLOWING STARTS. BEGIN PLOWING ON WINDWARD SIDE OF SITE. CHISEL-TYPE PLOWS SPACED ABOUT 12 INCHES APART, AND SPRING-TOOTHED HARROWS ARE EXAMPLES OF EQUIPMENT WHICH MAY PRODUCE THE DESIRED EFFECT.

SPRINKLING - SITE IS SPRINKLED UNTIL SURFACE IS WET.

BARRIERS - SOLID BOARD FENCES, SNOW FENCES, BURLAP FENCES, CRATE WALLS, BALES OF HAY, AND SIMILAR MATERIAL CAN BE USED TO CONTROL AIR CURRENTS AND SOIL BLOWING.

CALCIUM CHLORIDE - SHALL BE IN THE FORM OF LOOSE, DRY GRANULES OF FLAKES FINE ENOUGH TO FEED THROUGH COMMONLY USED SPREADERS AT A RATE THAT WILL KEEP SURFACE MOIST BUT NOT CAUSE POLLUTION OR PLANT DAMAGE. IF USED ON STEEPER SLOPES, THEN USE OTHER PRACTICES TO PREVENT WASHING INTO STREAMS OR ACCUMULATION AROUND PLANTS.

STONE - COVER SURFACE WITH CRUSHED STONE OR COARSE GRAVEL.

SOIL DE-COMPACTION TESTING REQUIREMENTS

- SUBGRADE SOILS PRIOR TO THE APPLICATION OF TOPSOIL (SEE PERMANENT SEEDING AND STABILIZATION NOTES FOR TOPSOIL REQUIREMENTS) SHALL BE FREE OF EXCESSIVE COMPACTION TO A DEPTH OF 6.0 INCHES TO ENHANCE THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER.
- AREAS OF THE SITE WHICH ARE SUBJECT TO COMPACTION TESTING AND/OR MITIGATION ARE GRAPHICALLY DEMONSTRATED ON THE CERTIFIED SOIL EROSION CONTROL PLAN.

- COMPACTION TESTING LOCATIONS ARE DENOTED ON THE PLAN. A COPY OF THE PLAN OR PORTION OF THE PLAN SHALL BE USED TO MARK LOCATIONS OF TESTS, AND ATTACHED TO THE COMPACTION REMEDIATION FORM, AVAILABLE FROM THE LOCAL SOIL CONSERVATION DISTRICT. THIS FORM MUST BE FILLED OUT AND SUBMITTED PRIOR TO RECEIVING A CERTIFICATE OF COMPLIANCE FROM THE DISTRICT.
- IN THE EVENT THAT TESTING INDICATES COMPACTION IN EXCESS OF THE MAXIMUM THRESHOLDS INDICATED FOR THE SIMPLIFIED TESTING METHODS (SEE DETAILS BELOW), THE CONTRACTOR/OWNER SHALL HAVE THE OPTION TO PERFORM EITHER (1) COMPACTION MITIGATION OVER THE ENTIRE MITIGATION AREA DENOTED ON THE PLAN (EXCLUDING EXEMPT AREAS), OR (2) PERFORM ADDITIONAL MORE DETAILED TESTING TO ESTABLISH THE LIMITS OF EXCESSIVE COMPACTION WHEREUPON ONLY THE EXCESSIVELY COMPACTED AREAS WOULD REQUIRE COMPACTION MITIGATION. ADDITIONAL DETAILED TESTING SHALL BE PERFORMED BY A TRAINED, LICENSED PROFESSIONAL.

- COMPACTION TESTING METHODS
- PROBING WIRE TEST (SEE DETAIL)
  - HAND-HELD PENETROMETER TEST (SEE DETAIL)
  - TUBE BULK DENSITY TEST (LICENSED PROFESSIONAL ENGINEER REQUIRED)
  - NUCLEAR DENSITY TEST (LICENSED PROFESSIONAL ENGINEER REQUIRED)

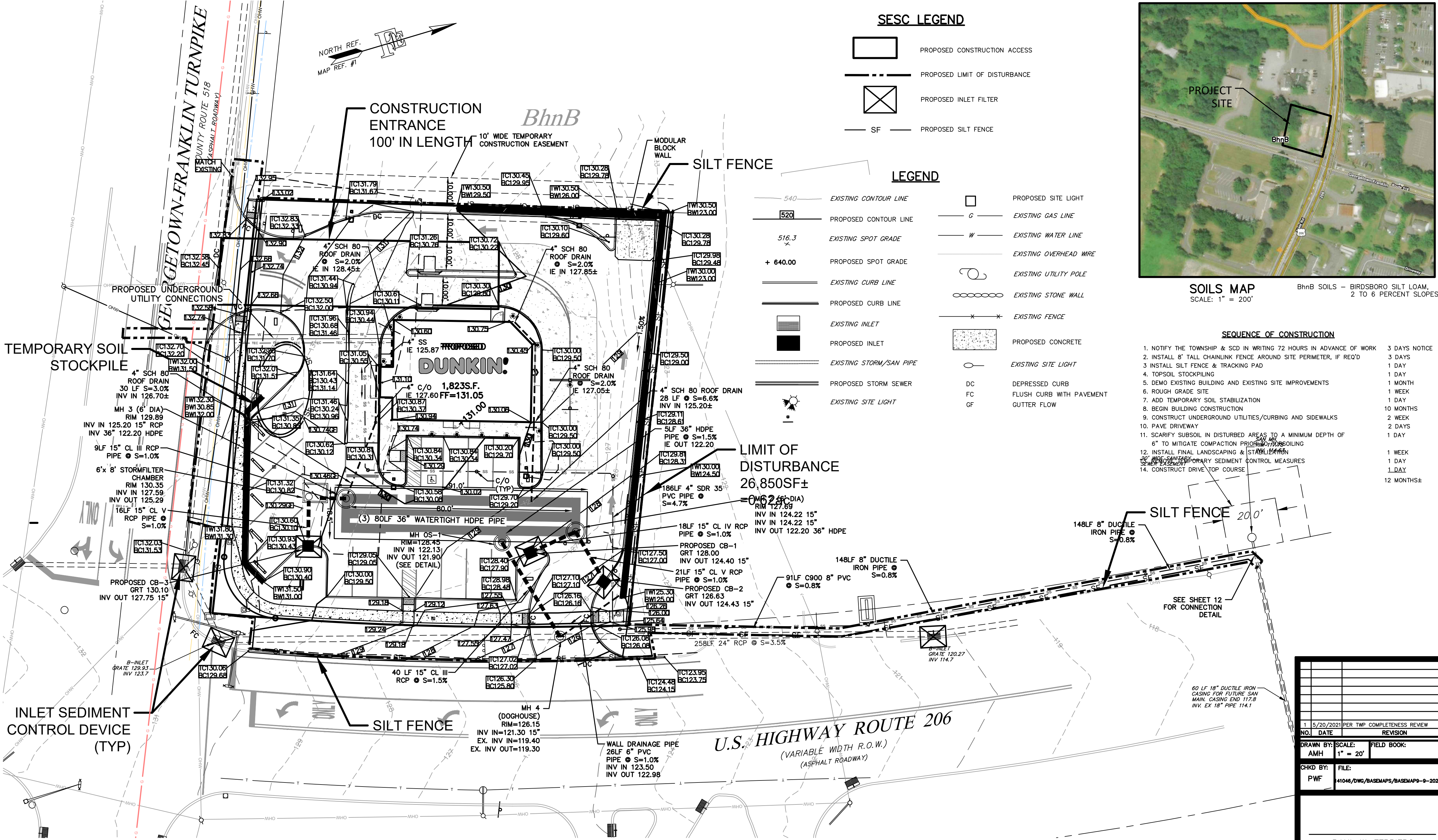
NOTE: ADDITIONAL TESTING METHODS WHICH CONFORM TO ASTM STANDARDS AND SPECIFICATIONS, AND WHICH PRODUCE A DRY WEIGHT, SOIL BULK DENSITY MEASUREMENT MAY BE ALLOWED SUBJECT TO DISTRICT APPROVAL.

SOIL COMPACTION TESTING IS NOT REQUIRED IF WHEN SUBSOIL COMPACTION REMEDIATION (SCARIFICATION/TILLAGE (6" MINIMUM DEPTH) OR SIMILAR) IS PROPOSED AS PART OF THE SEQUENCE OF CONSTRUCTION.

PROCEDURES FOR SOIL COMPACTION MITIGATION

PROCEDURES SHALL BE USED TO MITIGATE EXCESSIVE SOIL COMPACTION PRIOR TO PLACEMENT OF TOPSOIL AND ESTABLISHMENT OF PERMANENT VEGETATIVE COVER.

RESTORATION OF COMPACTED SOILS SHALL BE THROUGH DEEP SCARIFICATION/TILLAGE (6" MINIMUM DEPTH) WHERE THERE IS NO DANGER TO UNDERGROUND UTILITIES (CABLES, IRRIGATION SYSTEMS, ETC.). IN THE ALTERNATIVE, ANOTHER METHOD AS SPECIFIED BY A NEW JERSEY LICENSED PROFESSIONAL ENGINEER MAY BE SUBSTITUTED SUBJECT TO DISTRICT APPROVAL.

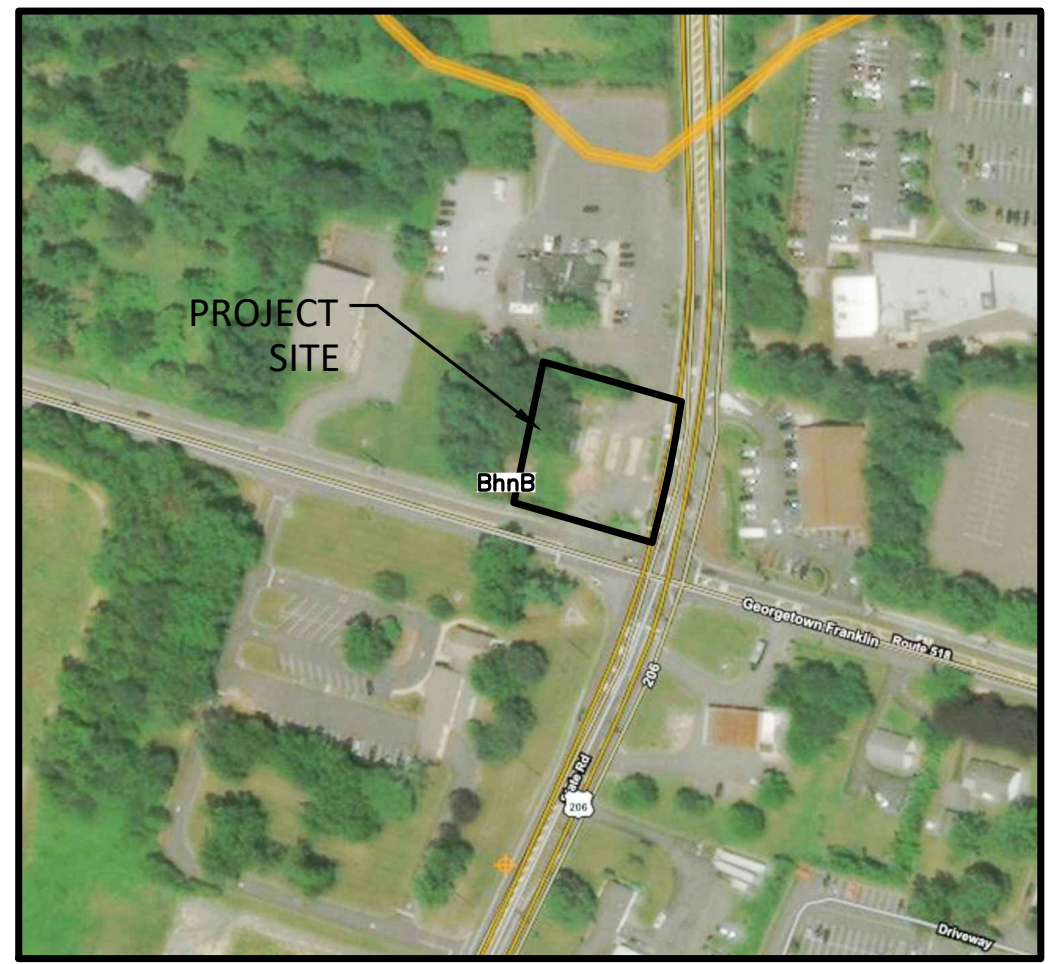


SESC LEGEND

- PROPOSED CONSTRUCTION ACCESS
- PROPOSED LIMIT OF DISTURBANCE
- PROPOSED INLET FILTER
- PROPOSED SILT FENCE

LEGEND

- EXISTING CONTOUR LINE
- PROPOSED CONTOUR LINE
- EXISTING SPOT GRADE
- PROPOSED SPOT GRADE
- EXISTING CURB LINE
- PROPOSED CURB LINE
- EXISTING INLET
- PROPOSED INLET
- EXISTING STORM/SAN PIPE
- PROPOSED STORM SEWER
- EXISTING SITE LIGHT
- PROPOSED SITE LIGHT
- EXISTING GAS LINE
- EXISTING WATER LINE
- EXISTING OVERHEAD WIRE
- EXISTING UTILITY POLE
- EXISTING STONE WALL
- EXISTING FENCE
- PROPOSED CONCRETE
- DEPRESSED CURB
- FLUSH CURB WITH PAVEMENT
- GUTTER FLOW



SOILS MAP  
SCALE: 1" = 200'

BhnB SOILS - BIRDSBORO SILT LOAM,  
2 TO 6 PERCENT SLOPES

SEQUENCE OF CONSTRUCTION

- NOTIFY THE TOWNSHIP & SCD IN WRITING 72 HOURS IN ADVANCE OF WORK
- INSTALL 8' TALL CHAINLINK FENCE AROUND SITE PERIMETER, IF REQ'D
- INSTALL SILT FENCE & TRACKING PAD
- TOPSOIL STOCKPILING
- DEMO EXISTING BUILDING AND EXISTING SITE IMPROVEMENTS
- ROUGH GRADE SITE
- ADD TEMPORARY SOIL STABILIZATION
- BEGIN BUILDING CONSTRUCTION
- CONSTRUCT UNDERGROUND UTILITIES/CURBING AND SIDEWALKS
- PAVE DRIVEWAY
- SCARIFY SUBSOIL IN DISTURBED AREAS TO A MINIMUM DEPTH OF 6" TO MITIGATE COMPACTION PROBLEMS
- INSTALL FINAL LANDSCAPING & STABILIZATION
- CONSTRUCT TEMPORARY SEDIMENT CONTROL MEASURES
- CONSTRUCT DRIVE TOP COURSE

1	5/20/2021	PWP	COMPLETENESS REVIEW	3 DAYS NOTICE
NO.	DATE	REVISION		
AMH	SCALE:	FIELD BOOK:		
PWF	FILE:			
	41046/DWG/BASEMAPS/BASEMAPS-9-2020			

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978

**Ferriero Engineering, Inc.**

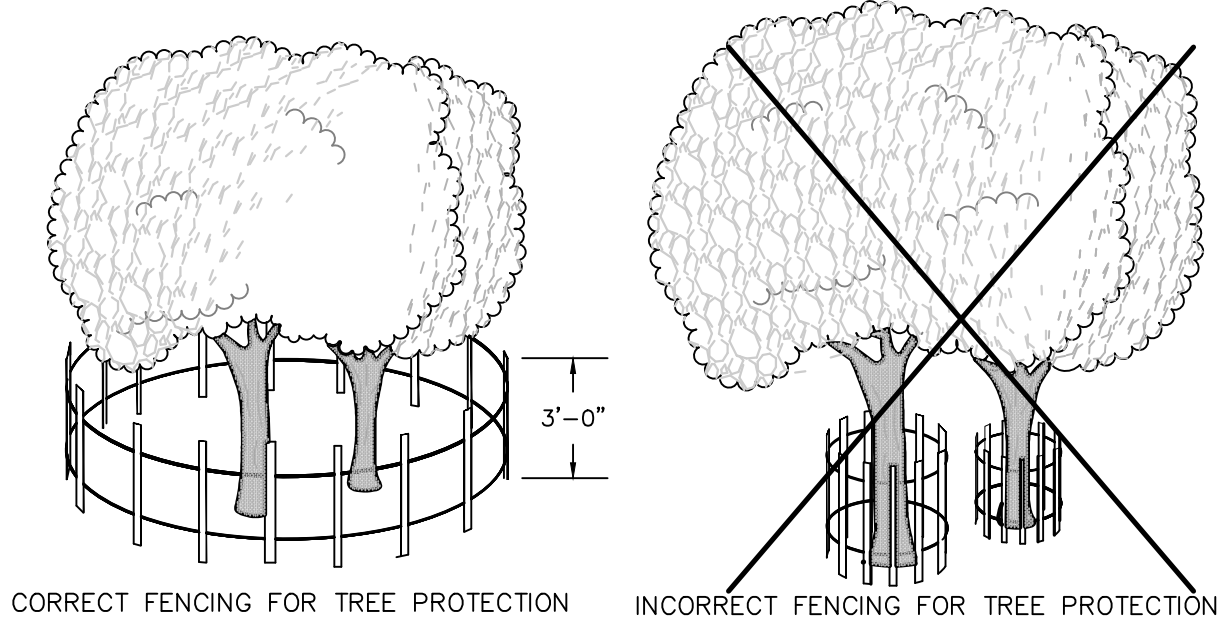
180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209  
CERTIFICATE OF AUTHORIZATION 24CA27935400

SOIL EROSION AND SEDIMENT CONTROL PLAN  
**DUNKIN'**

LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET  
6 OF 13

DATE: 2/25/2021 PROJECT NO:  
REVISION: 141046



TREE PROTECTION  
NOT TO SCALE

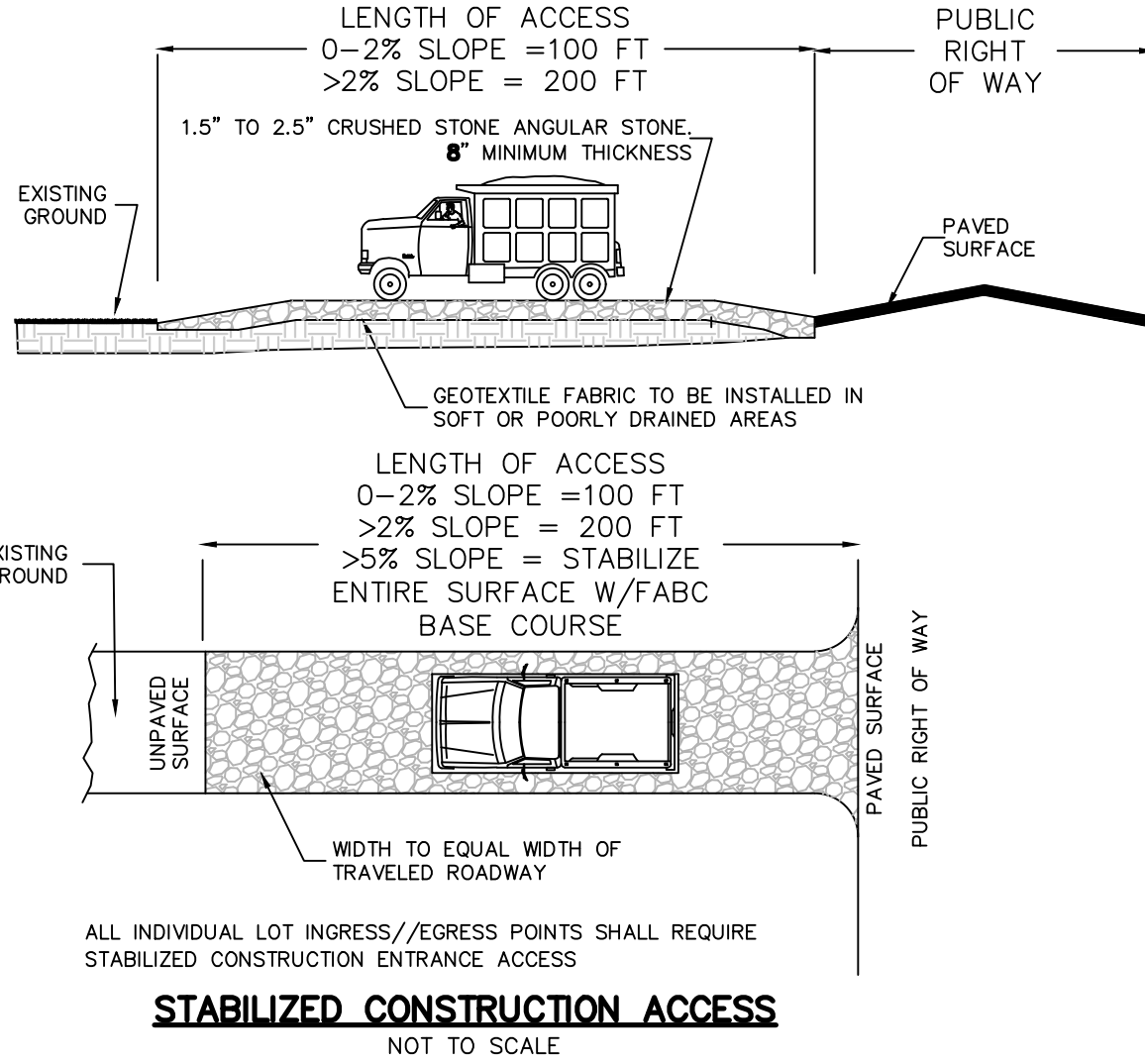
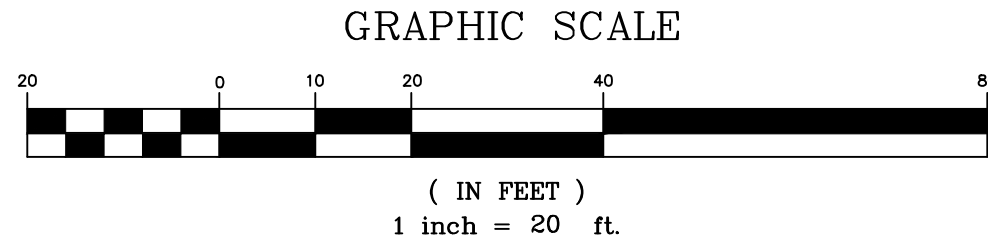
NOTE:

THE EXISTING UNDERGROUND UTILITY INFORMATION SHOWN ON THESE PLANS IS APPROXIMATE AND IS NOT GUARANTEED AS TO ACCURACY OR COMPLETENESS. PRIOR TO ANY CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL INFORMATION. THE CONTRACTOR SHALL CALL UNDERGROUND UTILITIES 1 (800) 272-1000 FOR MARKOUT PRIOR TO EXCAVATION. TEST PITS SHALL BE EXCAVATED BY THE CONTRACTOR PRIOR TO CONSTRUCTION TO ASCERTAIN EXISTING LOCATIONS, ELEVATIONS, MATERIALS, AND SIZES. TEST PIT DATA SHALL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION TO PERMIT ADJUSTMENTS AS REQUIRED.

NOTE:

ALL CONSTRUCTION SHALL CONFORM TO THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

SOIL EROSION & SEDIMENT CONTROL PLAN



INLET SEDIMENT CONTROL DEVICE

NOT TO SCALE

SILT FENCE

NOT TO SCALE



### 3-Loop Wave Style Bike Rack - 5 Bike Capacity, Galvanized



Upscale stylish look for downtown shopping and business districts.

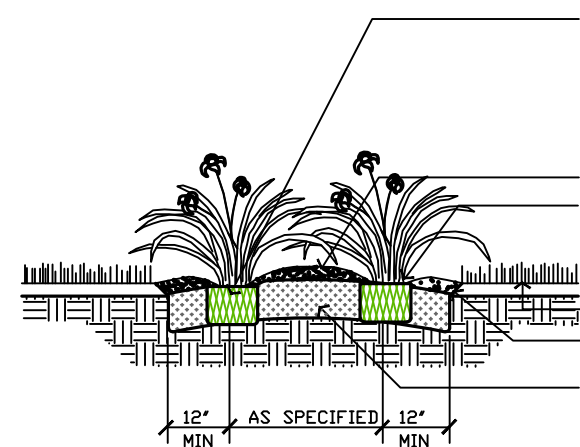
- For stadiums, parks and athletic fields.
- 10-gauge galvanized steel.
- 2 3/8" diameter bar.
- Bike rack mounting hardware included.

AS MANUFACTURED BY INLINE OR APPROVED EQUAL

MODEL NO.	DESCRIPTION	SIZE L x W x H	AVAILABLE COLORS
H-2543BLK	3-LOOP	43"x2 1/2" x 39"	BLACK GALVANIZED GREEN

### BICYCLE RACK

NOT TO SCALE



PLANT SHALL BE TRANSPLANTED AT THE SAME GRADE AS IT BORE IN THE NURSERY PLOT PRIOR TO PLANTING. INSPECT ROOTS AND PROVIDE VERTICAL CUTS ALONG ANY GIRDLING ROOTS WITHOUT LOOSENING ROOT MASS EXCESSIVELY.

3" DEEP WOOD CHIP MULCH SAUCER AROUND ALL PLANTS DO NOT COVER PLANT BASE WITH MULCH.

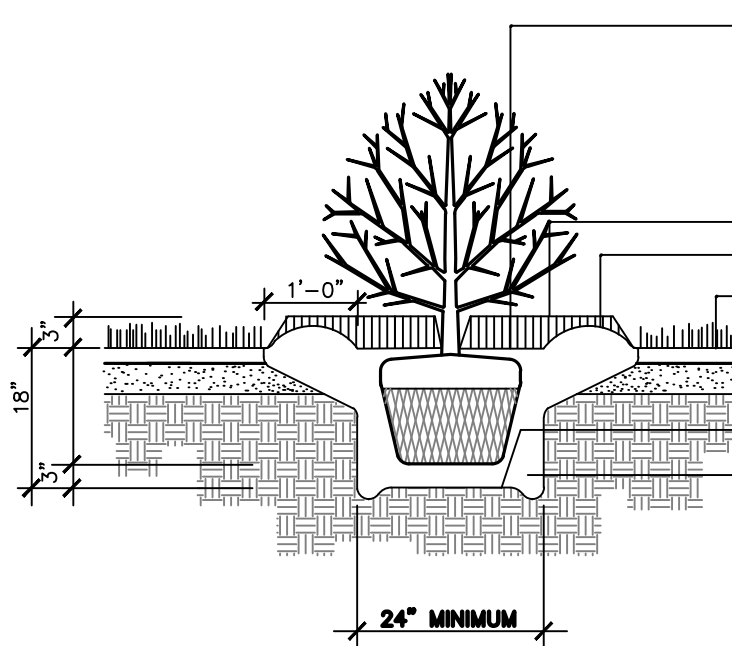
FINISHED GRADE  
CLEAN TROVELED EDGE

MOUNDING PLANTING BED  
6" DEPTH (MIN) ONE PART DRIED COV. MAURE, THREE PARTS HUMUS, ORGANIC TOPSOIL, FIRM MIXTURE WITH WATER PRIOR TO PLANTING.  
PLANTING MIXTURE WILL CHANGE WITH SOIL CONDITIONS.

FERTILIZER: 5-10-5, COMMERCIAL DESIGNATION, 1.5 POUNDS PER 100 SF APPLIED FOLLOWING SPRING PLANTING

### PERENNIAL HERBACEOUS PLANTING BED DETAIL

NOT TO SCALE



PLANT SHALL BE TRANSPLANTED AT THE SAME GRADE AS IT BORE IN THE NURSERY PLOT PRIOR TO BALLING AND BURLAPPING. CUT AND REMOVE BURLAP FROM TOP ONE-THIRD OF ROOT BALL AS SHOWN.

3" DEEP TRIPLE SHREDDED MULCH SAUCER

FINISHED GRADE  
SEE PLAN FOR ADJACENT MATERIALS

3" MOUND OF COMPACTED SOIL

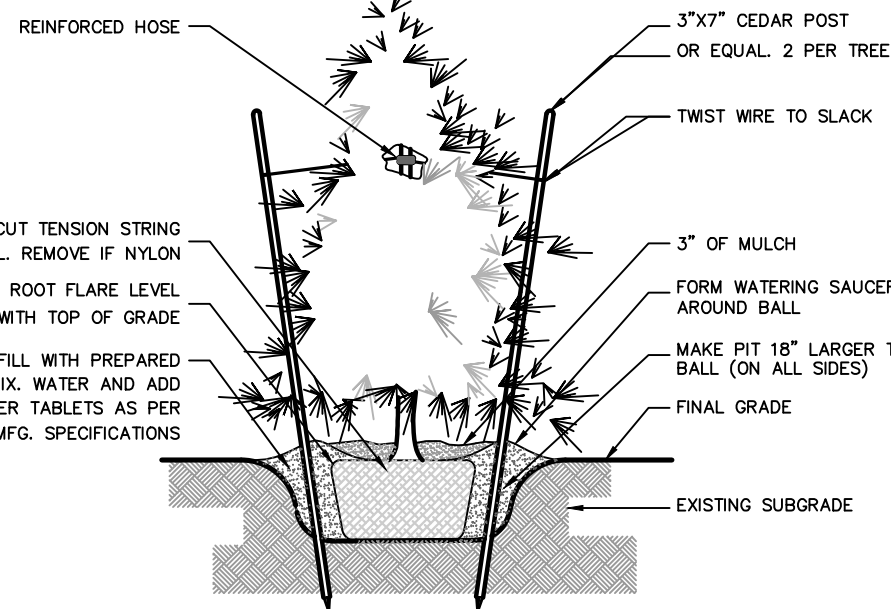
PLANTING BACKFILL MIX TO BE THOROUGH MIXTURE CONSISTING OF 50% SCREENED TOPSOIL, 25% HUMUS, 25% PARENT MATERIAL (15% PARENT MATERIAL/10% CERAMIC SOIL AMMENDMENT) AND TWO (2) FERTILIZER PACKS PER PLANT.

### SHRUB PLANTING DETAIL

NOT TO SCALE

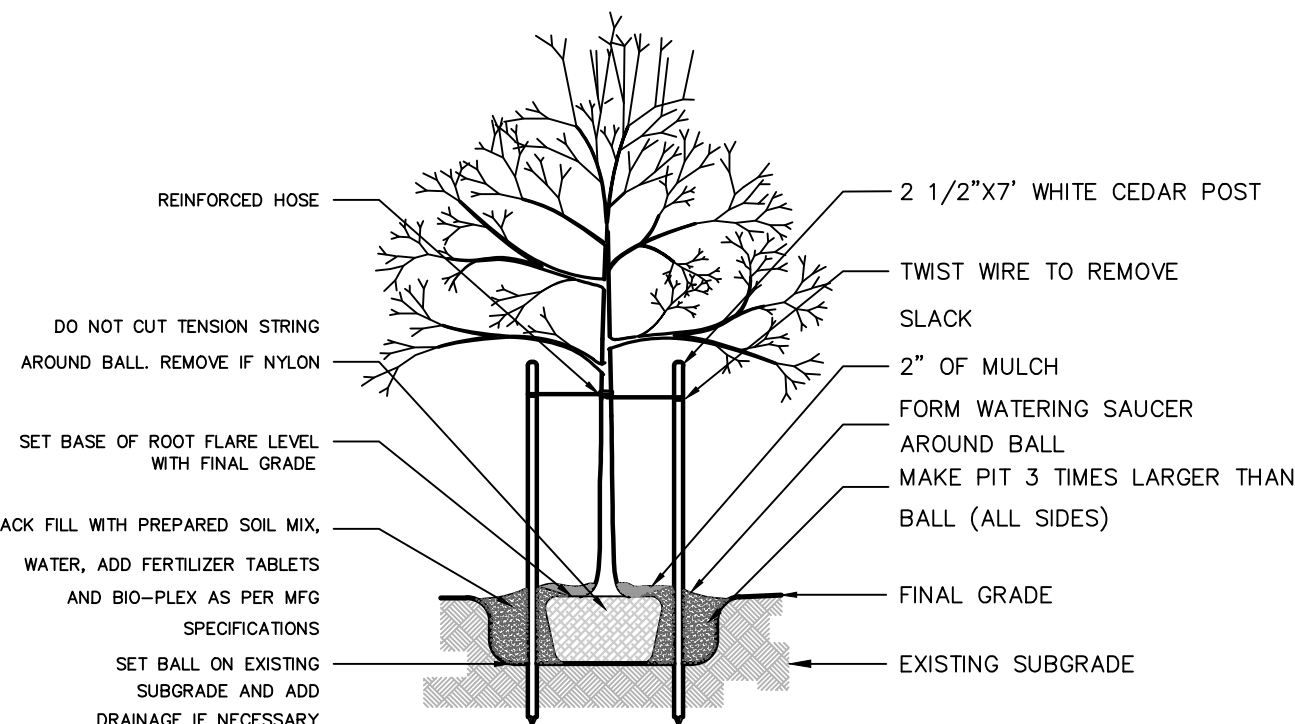
### IMPORTANT NOTES:

- CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY CODES. APPLICABLE SAFETY CODES SHALL MEAN THE LATEST EDITION INCLUDING ANY AND ALL AMENDMENTS, REVISIONS AND ADDITIONS THERETO OF THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION'S "OCCUPATIONAL SAFETY AND HEALTH STANDARDS (OSHA)," SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION" OF THE STATE OF NEW JERSEY, DEPARTMENT OF LABOR AND INDUSTRY, BUREAU OF ENGINEERING AND SAFETY, "CONSTRUCTION SAFETY CODE," AND "MAINTENANCE, CONSTRUCTION AND DEMOLITION," AND "BUILDING CODE."
- ALL SOIL TESTING FOR STRUCTURAL COMPONENTS TO BE DONE BY OTHERS. THIS INCLUDES BUT IS NOT LIMITED TO STRUCTURAL STABILITY, WATER TABLE ELEVATION, FOOTING DESIGN, PRESENCE OR ABSENCE OF BURIED OR UNSUITABLE MATERIAL AND DEPTH TO ROCK. SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM AFORESAID CONDITIONS.
- BUILDER/OWNER/CONTRACTOR OR AUTHORIZED REPRESENTATIVE TO APPROVE PROPOSED BUILDING ELEVATIONS AND LOCATION PRIOR TO ANY EXCAVATION. IF CHANGES ARE REQUESTED, THE SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM FIELD MODIFICATIONS.
- THE OWNER, BY AUTHORIZING FERRIERO ENGINEERING, INC. TO PERFORM CONSTRUCTION STAKE OUT OF ANY COMPONENT OF THE WORK HAS ACKNOWLEDGED THEIR REVIEW AND APPROVAL OF THE LOCATION AND ELEVATION OF ALL IMPROVEMENTS SHOWN.
- THESE PLANS ARE NOT VALID UNLESS SIGNED AND SEALED BY THE LICENSED PROFESSIONAL SHOWN HEREON.
- OWNER/APPLICANT/BUILDER/CONTRACTOR TO NOTIFY DESIGN ENGINEER IF ANY PLAN INCONSISTENCIES EXIST.
- CUT SHEETS FOR CURBING SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR REVIEW PRIOR TO INSTALLING CURBING.
- AN AS-BUILT PLAN CERTIFIED BY A PROFESSIONAL LAND SURVEYOR SHALL BE SUPPLIED AT THE COMPLETION OF CONSTRUCTION.
- A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.



### EVERGREEN TREE PLANTING DETAIL

NOT TO SCALE



### DECIDUOUS TREE PLANTING DETAIL

NOT TO SCALE

### LANDSCAPE PLAN

#### GRAPHIC SCALE



( IN FEET )  
1 inch = 20 ft.

### LANDSCAPE NOTE:

ALL PLANTINGS SHALL BE GUARANTEED FOR A 2 YEAR PERIOD AND ALL PLANTINGS SHALL COMPLY WITH THE AMERICAN STANDARDS FOR NURSERY STOCK (ANSIA60.1, 2014 EDITION) PREPARED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION. ALL AREAS NOT SPECIFIED FOR PLANTING SHALL EITHER RECEIVE A MINIMUM 4" DEEP LAYER OF TOPSOIL, FINE GRADED AND SEEDED AND HAYED FOR LAWN, OR FILTER FABRIC AND 3" DECORATIVE RIVER STONE INSTALLED TO A DEPTH TWICE THE STONE SIZE.

### LEGEND

540	EXISTING CONTOUR LINE
520	PROPOSED CONTOUR LINE
516.3	EXISTING SPOT GRADE
+ 640.00	PROPOSED SPOT GRADE
	EXISTING CURB LINE
	PROPOSED CURB LINE
	EXISTING INLET
	PROPOSED INLET
	EXISTING STORM/SAN PIPE
	PROPOSED STORM SEWER
	EXISTING SITE LIGHT
	PROPOSED SITE LIGHT
G	EXISTING GAS LINE
W	EXISTING WATER LINE
	EXISTING OVERHEAD WIRE
	EXISTING UTILITY POLE
	EXISTING STONE WALL
	EXISTING FENCE
	PROPOSED CONCRETE
	EXISTING SITE LIGHT

U.S. HIGHWAY ROUTE 206  
(VARIABLE WIDTH R.O.W.)  
(ASPHALT ROADWAY)

### Planting List

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE
AR	5	ACER RUBRUM	RED MAPLE TREE	6'-7', B&B
ABS	2	AMELANCHIER x GRANDIFLORA	AUTUMN BRILLIANCE SERVICEBERRY	1 Qt., CONT., 24" O.C.
ARB	24	THUJA OCCIDENTALIS	EMERALD GREEN ARBORVITAE	6'-7', B&B
SPI	15	SPIREA BUMALDA 'ANTHONY WATERER'	ANTHONY WATERER SPIREA	24"-36", #3 CAN, 3' O.C.
PFG	7	POTENTILLA FRUTICOSA 'GOLD STAR'	GOLD STAR POTENTILLA	24"-36", #3 CAN, 3' O.C.
LFZ	10	LEUCOTHOE FONTANESIANA 'ZEBLID'	SCARLETTA FETTERBUSH	12"-24", #3 CAN, 3' O.C.
BSU	24	BUXUS 'GREEN VELVET'	GREEN VELVET BOXWOOD	24"-36", #3 CAN, 3' O.C.
BCS	5	SEDUM SPURUM 'FULDA GLOW'	BRONZE CARPET STONECROP	1 Qt., CONT., 18" O.C.
BLU	32	JUNIPERUS HORIZONTALIS 'BLUE RUG'	BLUE RUG JUNIPER	1 Qt., CONT., 24" O.C.
CAR	23	CAREX TESTACEA	ORANGE SEDGE	1 Qt., CONT., 12" O.C.
PEN	30	PENNISETUM ALOPECUROIDES	HAMELN DWARF FOUNTAIN GRASS	1 Qt., CONT., 12" O.C.
DFG	10	NARCISSUS 'FEBRUARY GOLD'	FEBRUARY GOLD DAFFODILS	1 Qt., CONT., 24" O.C.
DIF	10	NARCISSUS 'ICE FOLLIES'	ICE FOLLIES DAFFODILS	1 Qt., CONT., 24" O.C.

### NOTE:

ALL CONSTRUCTION SHALL CONFORM TO THE NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. CE2978

**Ferriero Engineering, Inc.**

180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24GA27935400

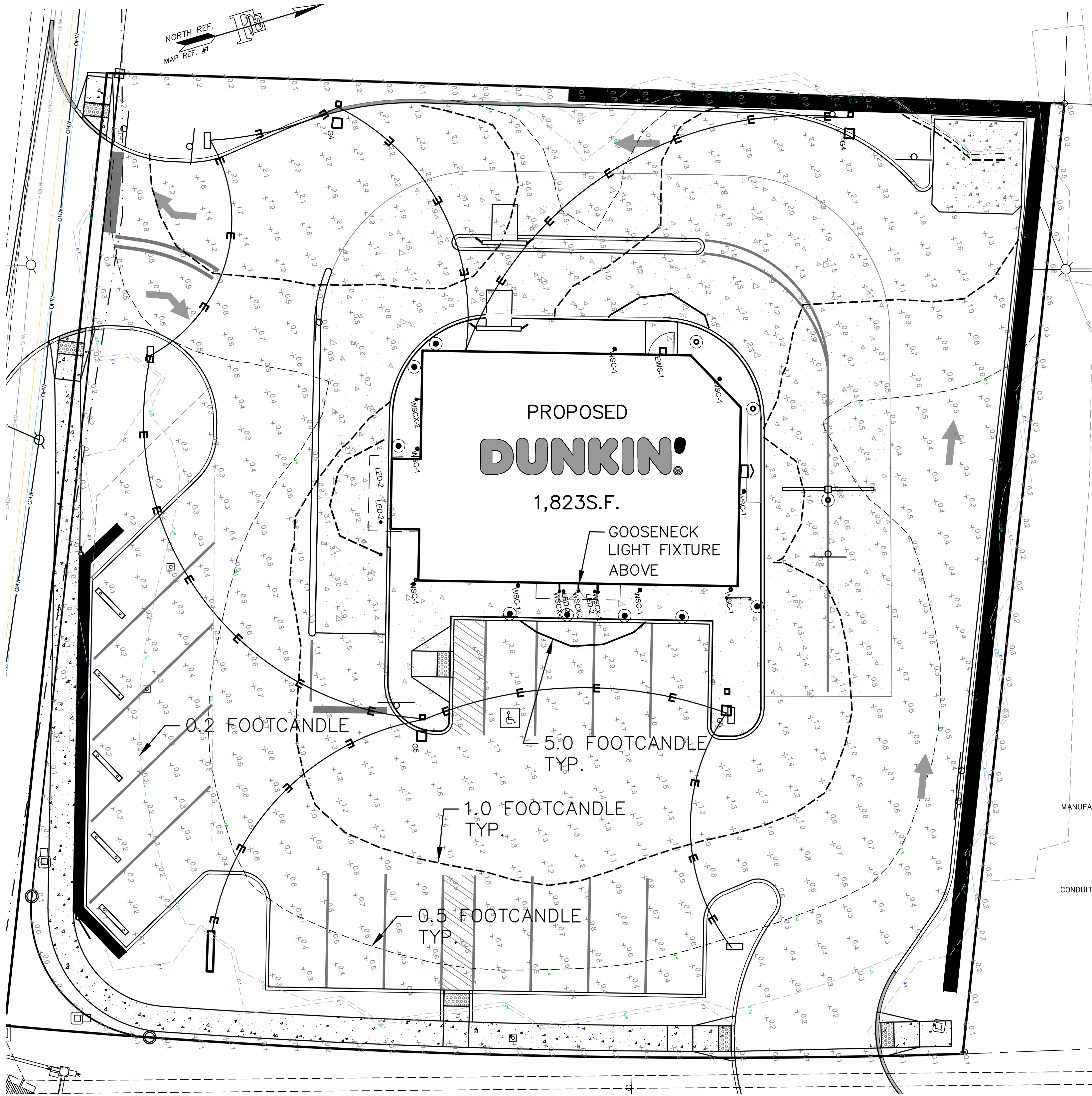
### LANDSCAPE PLAN

**DUNKIN'**

LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

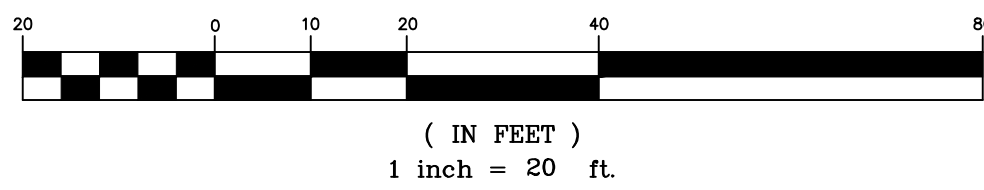
SHEET  
7 OF 13  
DATE: 2/25/2021 PROJECT NO:  
REVISION: 141046





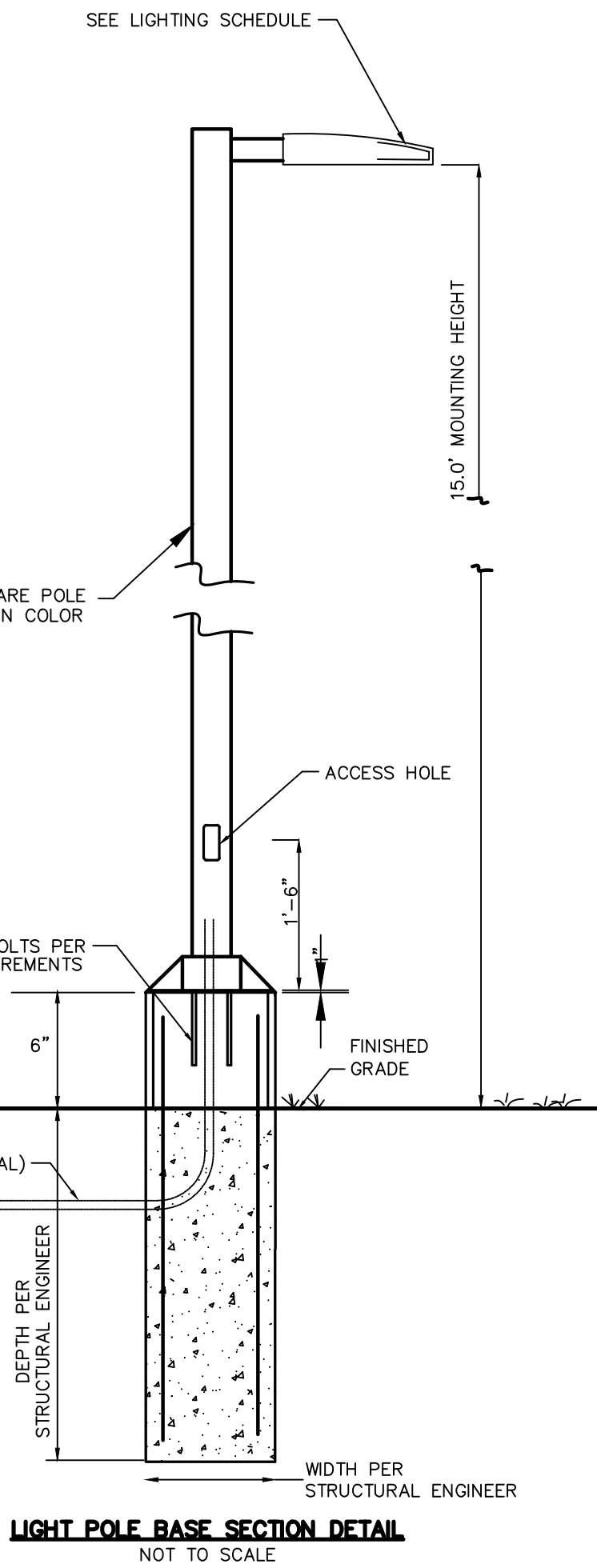
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  - A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.

**LIGHTING PLAN**  
GRAPHIC SCALE



**LIGHTING NOTES:**

- THIS LIGHTING PLAN DEPICTS PROPOSED SUSTAINED ILLUMINATION LEVELS CALCULATED USING DATA PROVIDED BY THE NOTED MANUFACTURER. ACTUAL SUSTAINED SITE ILLUMINATION LEVELS AND PERFORMANCE OF LUMINAIRES MAY VARY DUE TO VARIATIONS IN WEATHER, ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS, THE SERVICE LIFE OF EQUIPMENT AND LUMINAIRES AND OTHER RELATED VARIABLE FIELD CONDITIONS.
- THE LIGHTING VALUES AND CALCULATION POINTS DEPICTED ON THIS PLAN ARE ALL ANALYZED ON A HORIZONTAL GEOMETRIC PLANE AT ELEVATION ZERO (GROUND LEVEL) UNLESS OTHERWISE NOTED.
- THE LUMAIRES, LAMPS AND LENSES SHALL BE REGULARLY INSPECTED/MAINTAINED TO INSURE THAT THEY FUNCTION PROPERLY. THIS WORK MAY INCLUDE, BUT IS NOT LIMITED TO, VISUAL INSPECTIONS, CLEANING OF LENSES, AND RELAMPING (IF NECESSARY).
- THIS LIGHTING PLAN IS INTENDED TO SHOW THE LOCATIONS AND TYPE OF LUMINAIRES, POWER SYSTEM, CONDUITS, WIRING AND OTHER ELECTRICAL COMPONENTS ARE THE RESPONSIBILITY OF THE ARCHITECT AND YORK LIGHTING CONTRACTOR AS INDICATED IN THE CONSTRUCTION CONTRACT DOCUMENTS. THESE ITEMS SHALL BE INSTALLED AS REQUIRED BY STATE AND LOCAL REGULATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF LIGHTING FIXTURES AND APPURTENANCES IN ACCORDANCE WITH ALL APPLICABLE BUILDING AND ELECTRICAL CODES.
- ANY LIGHT LOCATIONS THAT CONFLICT WITH DRAINAGE, UTILITIES, OR OTHER STRUCTURES SHALL BE BROUGHT TO THE ATTENTION OF THE DESIGNER BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
- THE LIGHTING CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE CONTRACTOR REQUIREMENTS INDICATED IN THE SITE PLAN, INCLUDING BUT NOT LIMITED TO GENERAL NOTES, GRADING AND UTILITY NOTES, SITE SAFETY, AND GOVERNMENTAL REGULATIONS.
- MOUNTING HEIGHT FOR FREESTANDING LIGHTS SHALL BE NO HIGHER THAN FIFTEEN (15) FEET.
- OUTDOOR ILLUMINATION SHALL BE ON FROM DUSK TO DAWN UTILIZING PHOTOMETRIC CONTROLS BASED ON OPERATING HOURS.



Schedule					
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description
	G5	2	EATON - McGRAW-EDISON (FORMER COOPER LIGHTING)	GLEON-AF-01-LED-E1-SWQ-800-7030	GALLEON AREA AND ROADWAY LUMINAIRE (1) 70 CRI, 3000K, 800mA LIGHTSQUARE WITH 16 LEDS EACH AND TYPE V WIDE OPTICS
	G4	2	COOPER LIGHTING SOLUTIONS - McGRAW-EDISON (FORMERLY EATON)	GLEON-AF-01-LED-E1-SL4-800-HSS-7030	GALLEON AREA AND ROADWAY LUMINAIRE (1) 70 CRI, 3000K, 800mA LIGHTSQUARE WITH 16 LEDS EACH AND TYPE IV SPILL LIGHT ELIMINATOR OPTICS WITH HOUSE SIDE SHIELD
	EWS	1	WAC Lighting	SHARK(12) A032714	
	LED-2	4	DMF Lighting	DRD2M10930-DRD2TR45WH	N/A
	WSCX-2	4	HILITE		H-GU10-119/18-1-119
	WSC-1	8	PROGRESS	P5675	5" LED Cylinder 34W Direct/Indirect

**Statistics**

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Parking Lot	+	1.1 fc	10.2 fc	0.1 fc	102.0:1	11.0:1
Property Line	+	0.2 fc	0.7 fc	0.0 fc	N/A	N/A

Luminaires & Lamps Furnished By Villa Lighting Inc. St Louis, MO. 63103  
(800)325-0693  
www.villalighting.com

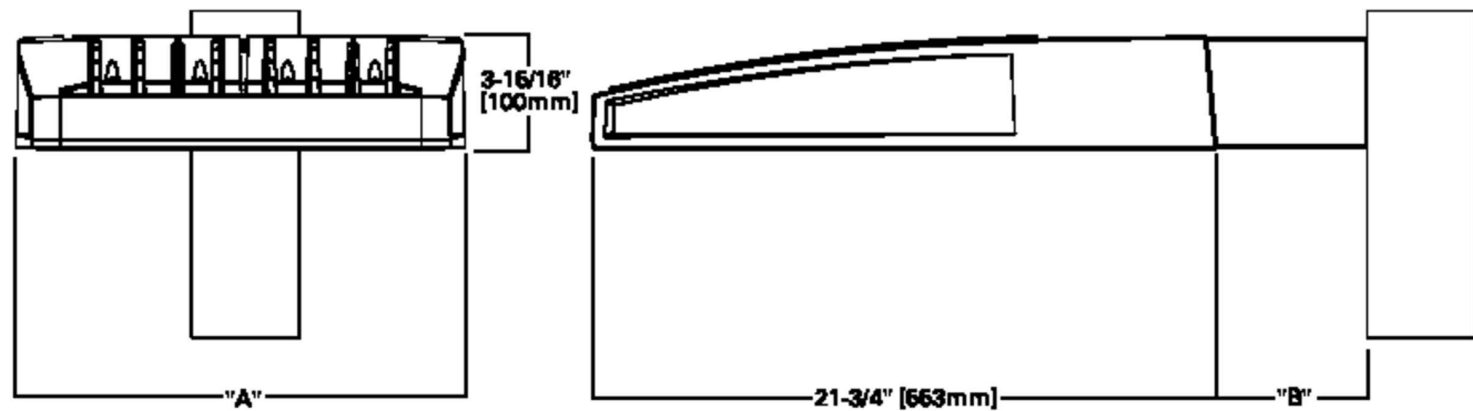
The electrical contractor shall be responsible for receiving, storage, installation and wiring of light fixtures.

The electrical contractor shall report any damaged light fixtures or missing parts to Villa Lighting within 48 hours of receipt of light fixture package.

Design is based on current information provided at the time of request. Any changes in mounting height, mounting location, lamp wattage, lamp type, and existing field conditions that effect any of the previously mentioned will void the current layout and require a change request and recalculation. Calculations are based upon a computer simulation and actual field calculations may vary.

Fixtures mounted on 15' pole  
Light level calculated on the ground

**DIMENSIONS**



**DIMENSION DATA**

Number of Light Squares	"A" Width	"B" Standard Arm Length	"B" Optional Arm Length	Weight with Arm (lbs.)	EPA with Arm (Sq. Ft.)
1-4	16-1/2" (394mm)	7" (178mm)	10" (254mm)	33 (16.2 lbs.)	0.96
6-8	21-5/8" (549mm)	7" (178mm)	10" (254mm)	44 (20.0 lbs.)	1.00
7-8	27-5/8" (702mm)	7" (178mm)	13" (330mm)	64 (24.6 lbs.)	1.07
9-10	33-3/4" (867mm)	7" (178mm)	16" (406mm)	83 (28.6 lbs.)	1.12

NOTE: 1. Optional arm length to be used when mounting two fixtures at 90° on a single pole. 2. EPA calculated with optional arm length.

McGRAW-EDISON GLEON LIGHT



DUNKIN' - ROUTE 203 - MONTGOMERY, NJ

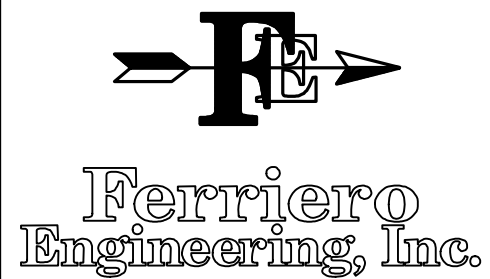
**LEGEND**

- 540 ——— EXISTING CONTOUR LINE
- 520 ——— PROPOSED CONTOUR LINE
- 516.3 ——— EXISTING SPOT GRADE
- + 640.00 ——— PROPOSED SPOT GRADE
- EXISTING CURB LINE
- PROPOSED CURB LINE
- EXISTING INLET
- PROPOSED INLET
- EXISTING STORM/SAN PIPE
- PROPOSED STORM SEWER
- EXISTING SITE LIGHT
- PROPOSED SITE LIGHT
- G ——— EXISTING GAS LINE
- W ——— EXISTING WATER LINE
- EXISTING OVERHEAD WIRE
- EXISTING UTILITY POLE
- EXISTING STONE WALL
- EXISTING FENCE
- PROPOSED CONCRETE
- EXISTING SITE LIGHT



1	5/20/2021	PER TWP COMPLETENESS REVIEW
NO.	DATE	REVISION
DRAWN BY:	SCALE:	FIELD BOOK:
AMH	1" = 20'	
CHKD BY:	FILE:	
PWF	41046/DWG/BASEMAPS/BASEMAPS-9-2020	

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. CE32978



180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24GA27935400

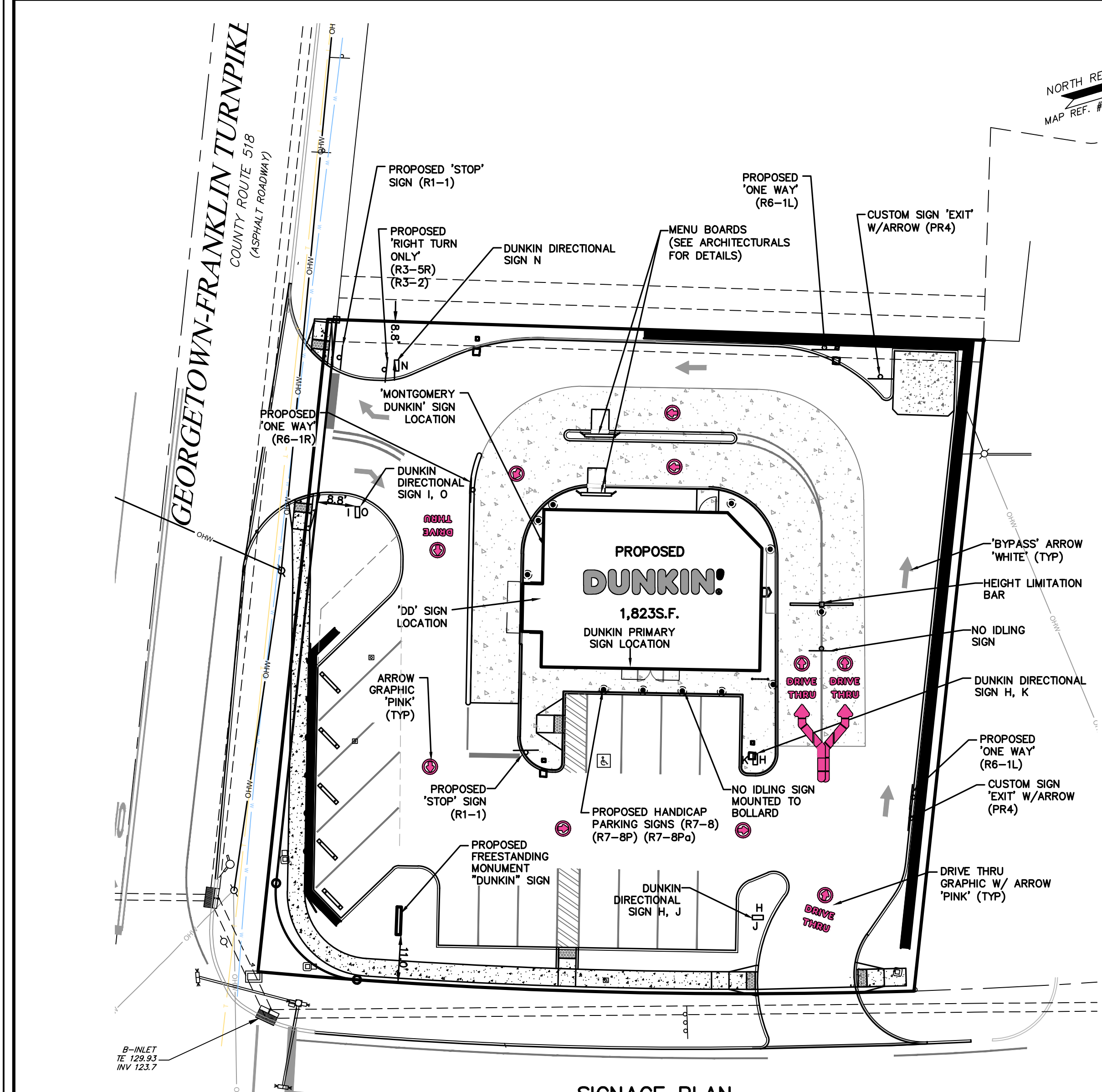
**LIGHTING PLAN**

**DUNKIN'!**  
LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET  
8 OF 13

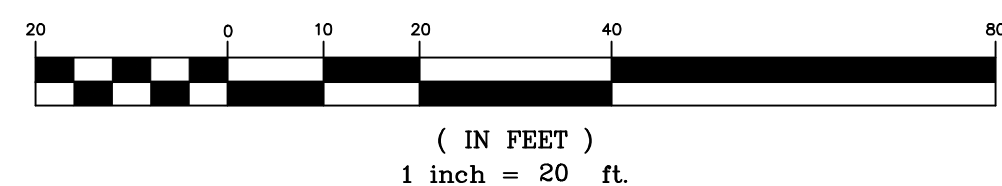
DATE: 2/25/2021 PROJECT NO:  
REVISION: 141046



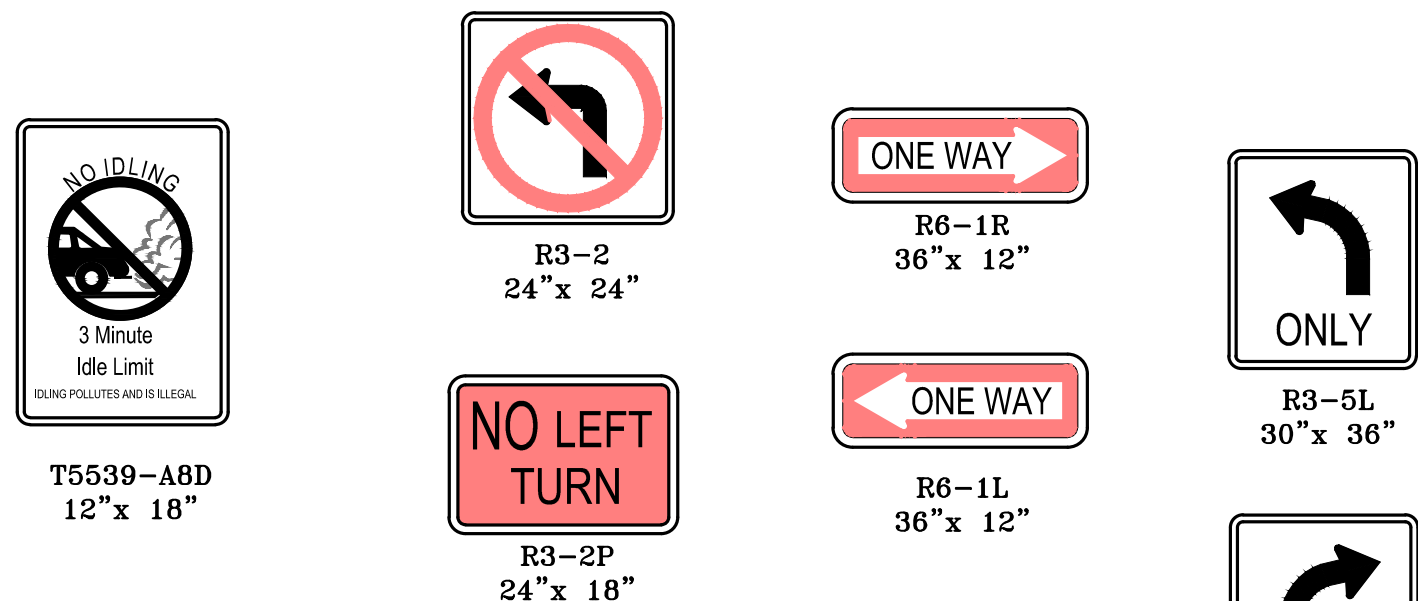


SIGNAGE PLAN

GRAPHIC SCALE

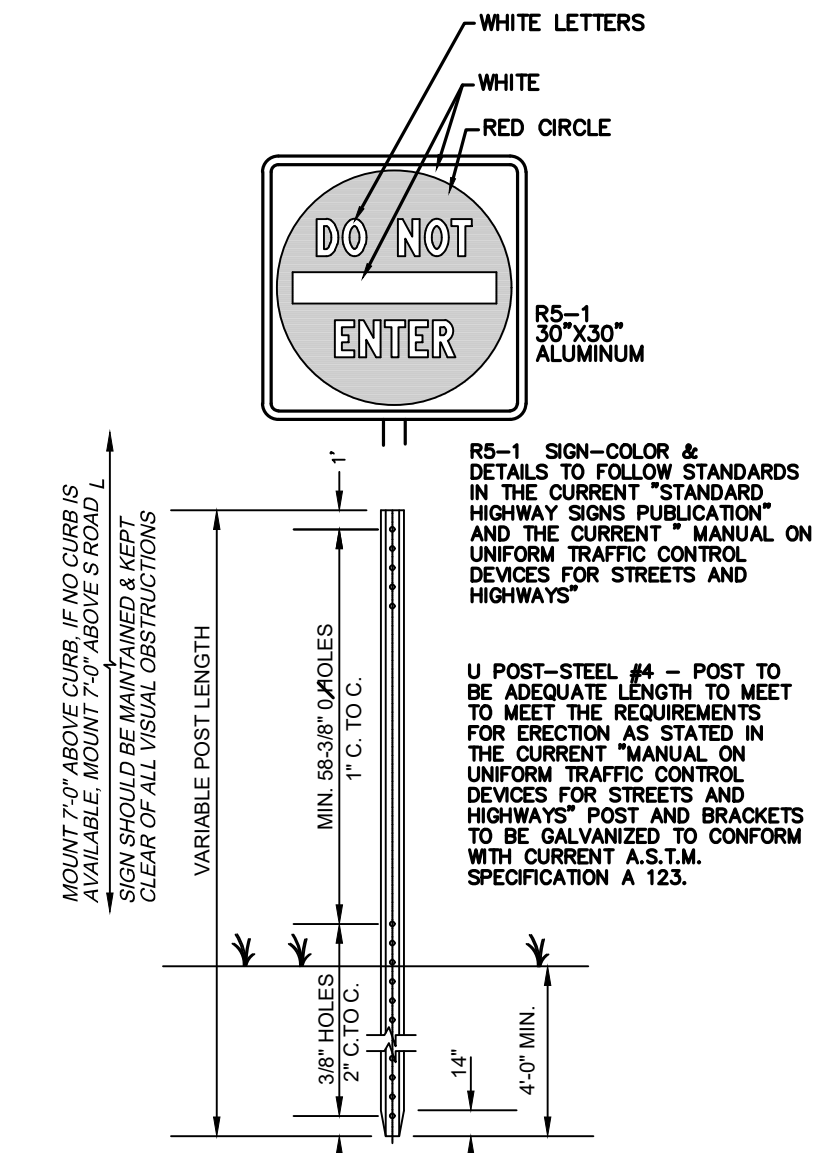


NOTE: BRICK USED FOR THE FREESTANDING SIGN SHALL BE THE SAME TYPE AND COLOR OF BRICK USED FOR THE BUILDING FACADE.



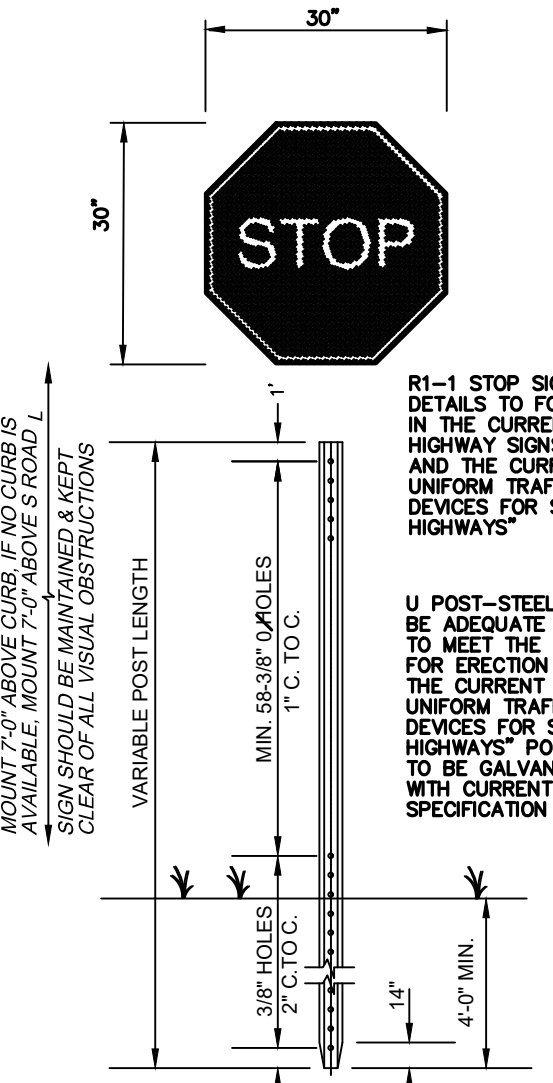
- IMPORTANT NOTES:**
- CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE SAFETY CODES. APPLICABLE SAFETY CODES SHALL MEAN THE LATEST EDITION INCLUDING ANY AND ALL AMENDMENTS, REVISIONS AND ADDITIONS THERETO OF THE FEDERAL DEPARTMENT OF LABOR, OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION'S "OCCUPATIONAL SAFETY AND HEALTH STANDARDS" (OSHA), "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION" OF THE STATE OF NEW JERSEY, DEPARTMENT OF LABOR AND INDUSTRY, BUREAU OF ENGINEERING AND SAFETY, "CONSTRUCTION SAFETY CODE," AND "MAINTENANCE, CONSTRUCTION AND DEMOLITION" AND "BUILDING CODE".
  - ALL SOIL TESTING FOR STRUCTURAL COMPONENTS TO BE DONE BY OTHERS. THIS INCLUDES BUT IS NOT LIMITED TO STRUCTURAL STABILITY, WATER TABLE ELEVATION, FOOTING DESIGN, PRESENCE OR ABSENCE OF BURIED OR UNSUITABLE MATERIAL AND DEPTH TO ROCK. SITE DESIGN ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO CHANGES OCCURRING FROM AFORESAID CONDITIONS.
  - BUILDER/OWNER/CONTRACTOR OR AUTHORIZED REPRESENTATIVE TO APPROVE PROPOSED BUILDING ELEVATIONS AND LOCATION PRIOR TO ANY EXCAVATION. IF CHANGES ARE REQUESTED, THE SITE DESIGN ENGINEER IS TO BE NOTIFIED IMMEDIATELY. THE SITE ENGINEER ACCEPTS NO LIABILITY FOR LOST TIME OR ADDITIONAL EXPENSE DUE TO ANY CHANGES OCCURRING FROM FIELD MODIFICATIONS.
  - THE OWNER, BY AUTHORIZING FERRIERO ENGINEERING, INC. TO PERFORM CONSTRUCTION STAKE OUT OF ANY COMPONENT OF THE WORK HAS ACKNOWLEDGED THEIR REVIEW AND APPROVAL OF THE LOCATION AND ELEVATION OF ALL IMPROVEMENTS SHOWN.
  - THESE PLANS ARE NOT VALID UNLESS SIGNED AND SEALED BY THE LICENSED PROFESSIONAL SHOWN HEREON.
  - OWNER/APPLICANT/BUILDER/CONTRACTOR TO NOTIFY DESIGN ENGINEER IF ANY PLAN INCONSISTENCIES EXIST.
  - CUT SHEETS FOR CURBING SHALL BE SUBMITTED TO THE DESIGN ENGINEER FOR REVIEW PRIOR TO INSTALLING CURBING.
  - AN AS-BUILT PLAN, CERTIFIED BY A PROFESSIONAL LAND SURVEYOR SHALL BE SUPPLIED AT THE COMPLETION OF CONSTRUCTION.
  - A NEW JERSEY PROFESSIONAL ENGINEER SHALL CERTIFY THE CONSTRUCTED IMPROVEMENTS ARE BUILT IN ACCORDANCE WITH THE APPROVED PLANS.

- SIGN NOTES:**
- 16-5.13 DIRECTIONAL SIGNS ARE PERMITTED, PROVIDED THEY DO NOT EXCEED TWO (2) SF.
  - PROPOSED BUILDING MOUNTED SIGNS TO EXTEND NO MORE THAN 6" FROM THE FACE OF THE STRUCTURE.
  - MENU BOARD DETAILS TO BE PROVIDED PRIOR TO ISSUANCE OF BUILDING PERMIT.



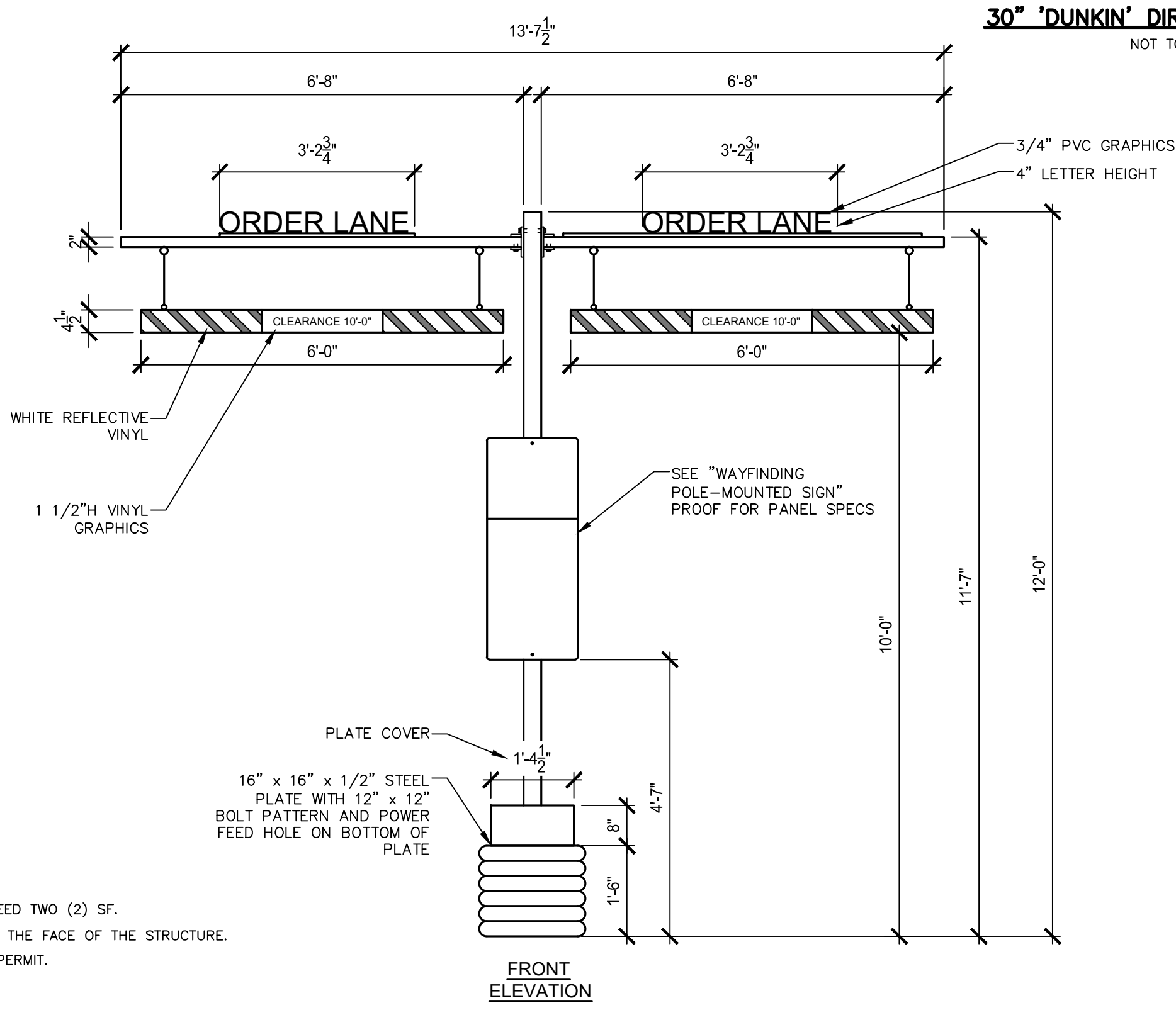
"DO NOT ENTER" SIGN DETAIL (R5-1)

NOT TO SCALE



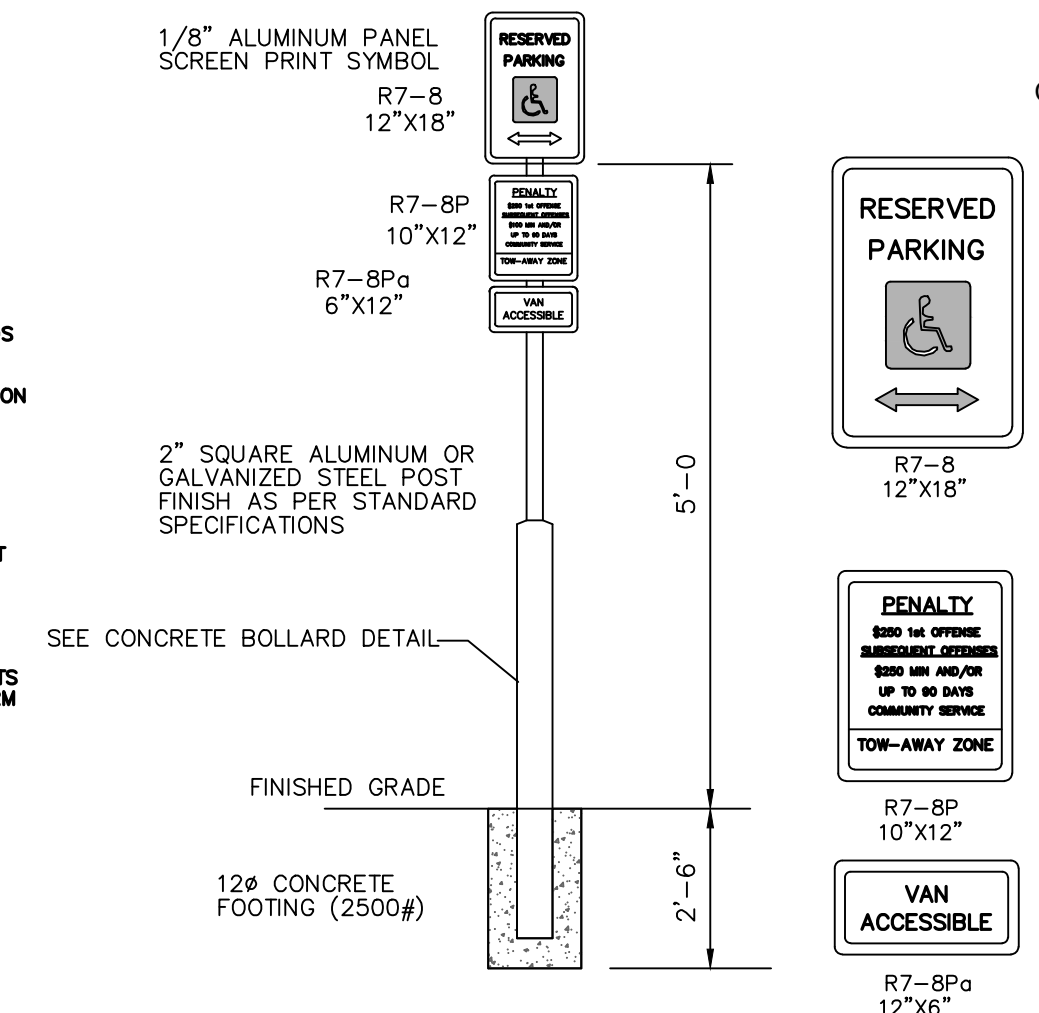
STREET STOP SIGN DETAIL (R1-1)

NOT TO SCALE



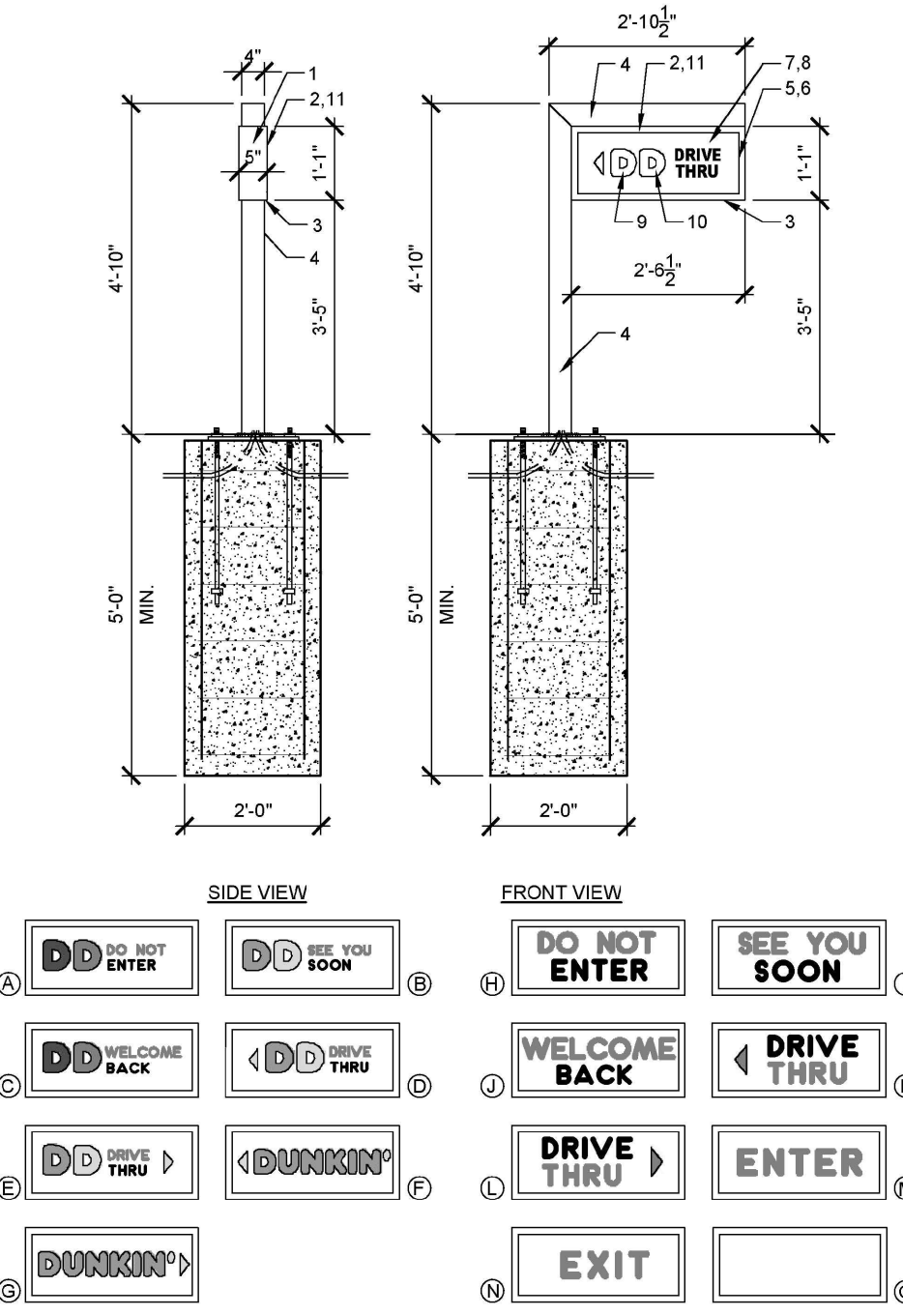
HEIGHT LIMITATION BAR DETAIL

NOT TO SCALE



ACCESSIBLE SIGN DETAILS

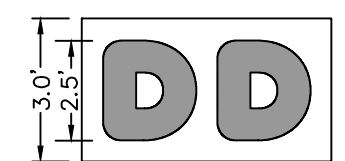
NOT TO SCALE



SIGN AREA= 12.5 SF  
NON-ILLUMINATED

20" MONTGOMERY DUNKIN' BUILDING SIGN

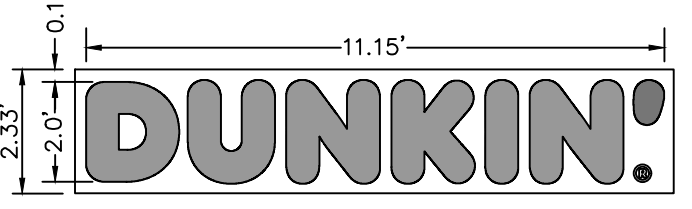
NOT TO SCALE



SIGN AREA= 17.6 SF  
EXTERNALLY ILLUMINATED

30" H 'DD' SECONDARY BUILDING SIGN

NOT TO SCALE



SIGN AREA= 26.8 SF  
EXTERNALLY ILLUMINATED

24" H 'DUNKIN' PRIMARY BUILDING SIGN

NOT TO SCALE

30" 'DUNKIN' DIRECTIONAL SIGNAGE

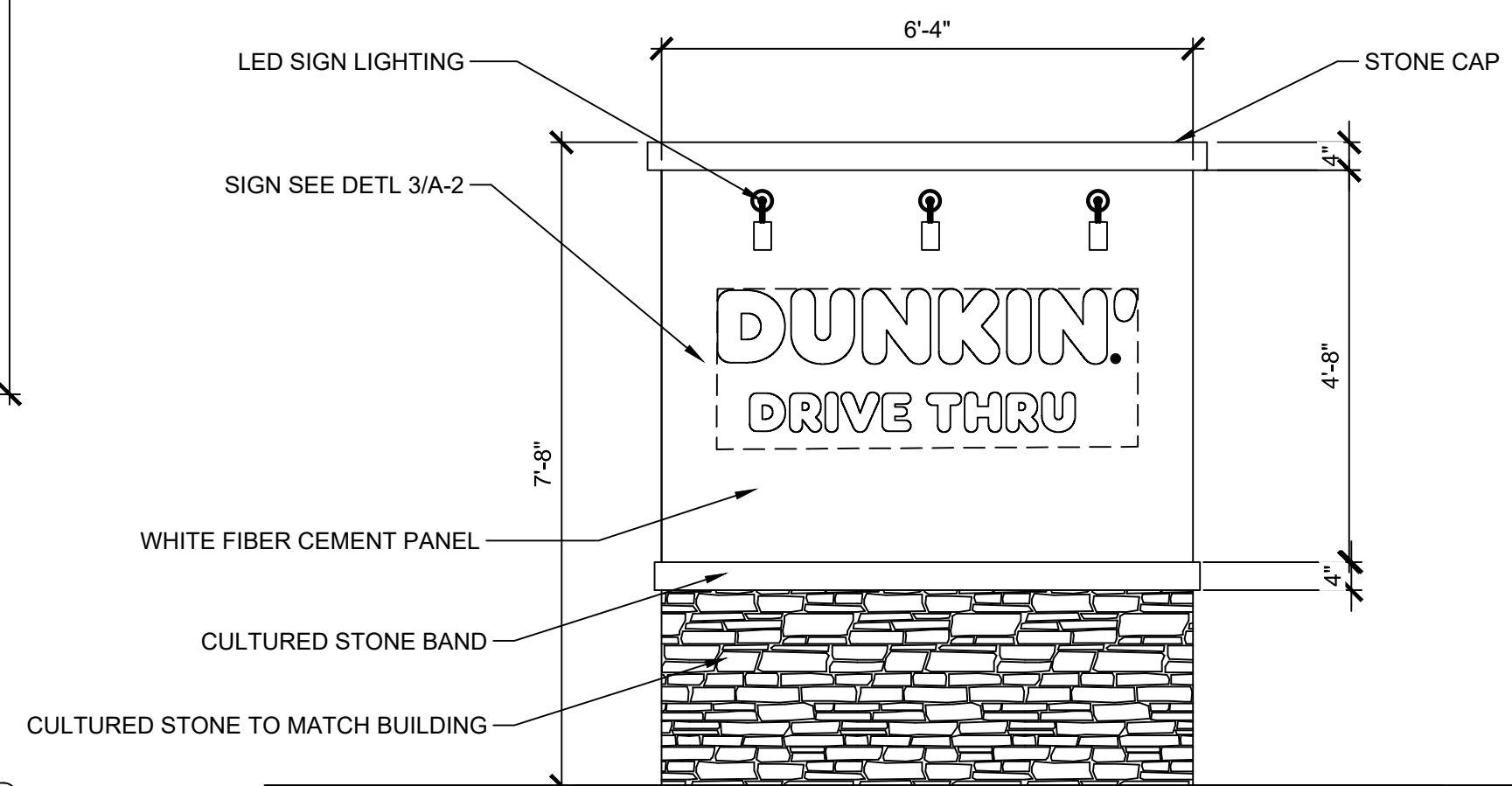
NOT TO SCALE

**GENERAL NOTES:**  
SEE ARCHITECTURAL PLANS BY GK+A ARCHITECTS, P.C. FOR COLORS.  
SEE ARCHITECTURAL PLANS FOR SIGNAGE MOUNTS TO BUILDING.  
SEE ARCHITECTURAL PLANS FOR BUILDING TREATMENTS.

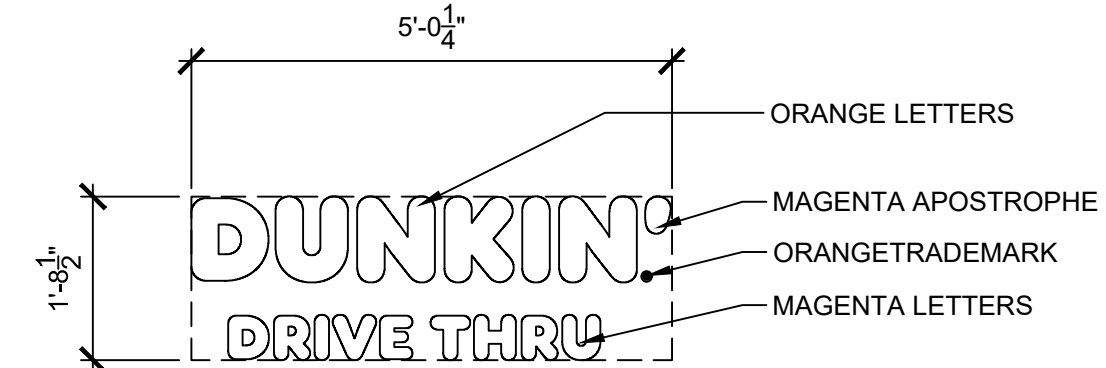
SIGN DATA

	REQUIRED	TOTAL NUMBER OF SIGNS	TOTAL AREA	MOUNTING HEIGHT
BUILDING MOUNTED	SIGN MAY NOT EXCEED 5% OF FACADE FACING STREET OR 50 SF (WHICHEVER SMALLER)	3 (1)	56.90 SF	13.5 FT
FREESTANDING	SIGN MAY NOT EXCEED 5% OF FRONT FACADE OR 75 SF (WHICHEVER SMALLER)	1 (1)	14.54 SF	7.67 FT

(1) ONE FREE STANDING SIGN AND BUILDING MOUNTED SIGN ALLOWED FOR PRINCIPAL BUILDING NOT PART OF A SHOPPING CENTER. ONE ADDITIONAL BUILDING MOUNTED SIGN FOR CORNER LOTS. AREA NOT TO EXCEED 1/2 SQUARE FOOT PER ONE LINEAR FOOT OF BUILDING FACADE FRONTING ON SAID STREET.



SIGNAGE AREA: 22.66 SQ. FT.  
ILLUMINATION TO BE WHITE LED'S



DUNKIN' MONUMENT SIGN DETAIL

SIGNAGE AREA: 8.5 SQ. FT.  
ILLUMINATION TO BE EXTERNAL LED

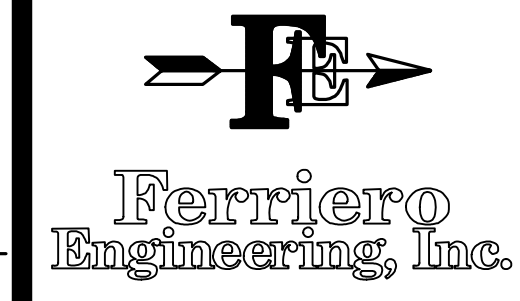
DUNKIN' MONUMENT SIGN DETAIL

NOT TO SCALE

- LEGEND**
- 540 EXISTING CONTOUR LINE
  - 520 PROPOSED CONTOUR LINE
  - 516.3 EXISTING SPOT GRADE
  - + 640.00 PROPOSED SPOT GRADE
  - EXISTING CURB LINE
  - PROPOSED CURB LINE
  - EXISTING INLET
  - PROPOSED INLET
  - EXISTING STORM/SAN PIPE
  - PROPOSED STORM SEWER
  - EXISTING SITE LIGHT
  - PROPOSED SITE LIGHT
  - EXISTING GAS LINE
  - EXISTING WATER LINE
  - EXISTING OVERHEAD WIRE
  - EXISTING UTILITY POLE
  - EXISTING STONE WALL
  - EXISTING FENCE
  - PROPOSED CONCRETE
  - EXISTING SITE LIGHT

1	5/20/2021	PER TWP COMPLETENESS REVIEW
NO.	DATE	REVISION
DRAWN BY:	SCALE:	FIELD BOOK:
AMH	1" = 20'	
CHKD BY:	FILE:	
PWF	41046/OWO/BASEMAPS/BASEMAP9-9-2020	

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978



180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209  
CERTIFICATE OF AUTHORIZATION 246A27935400

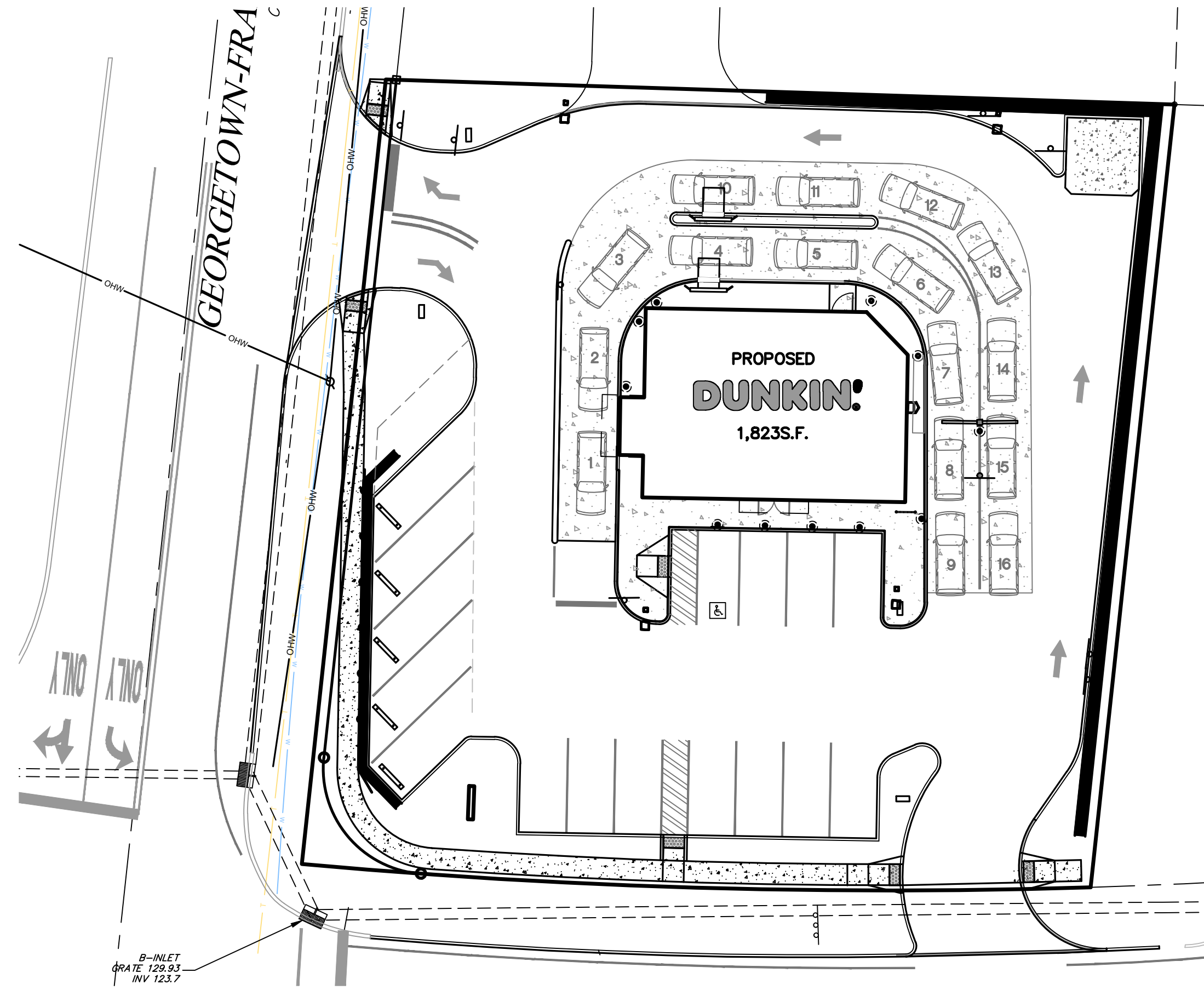
SIGNAGE PLAN



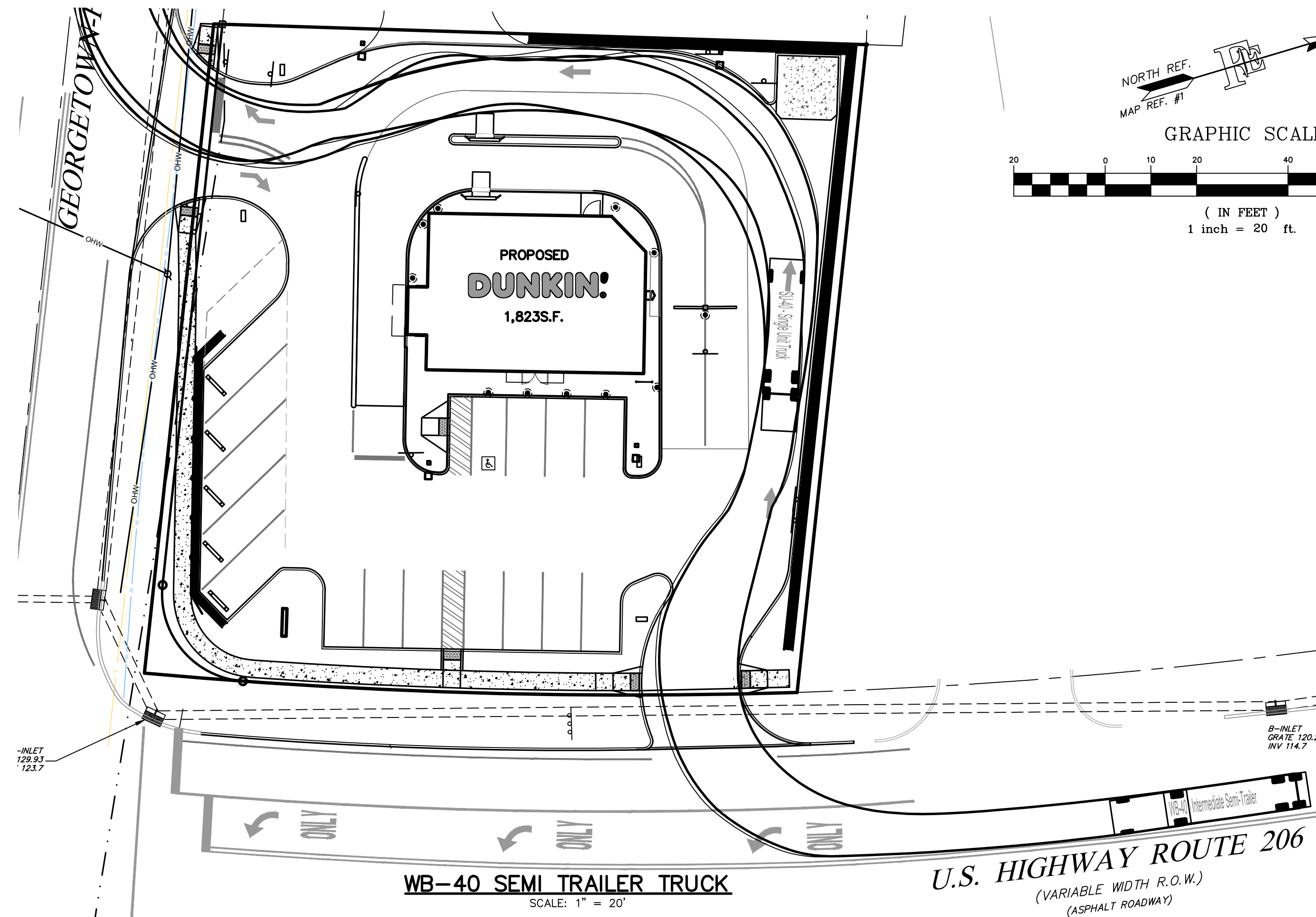
LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET	9 OF 13
DATE:	2/25/2021
REVISION:	PROJECT NO: 141046





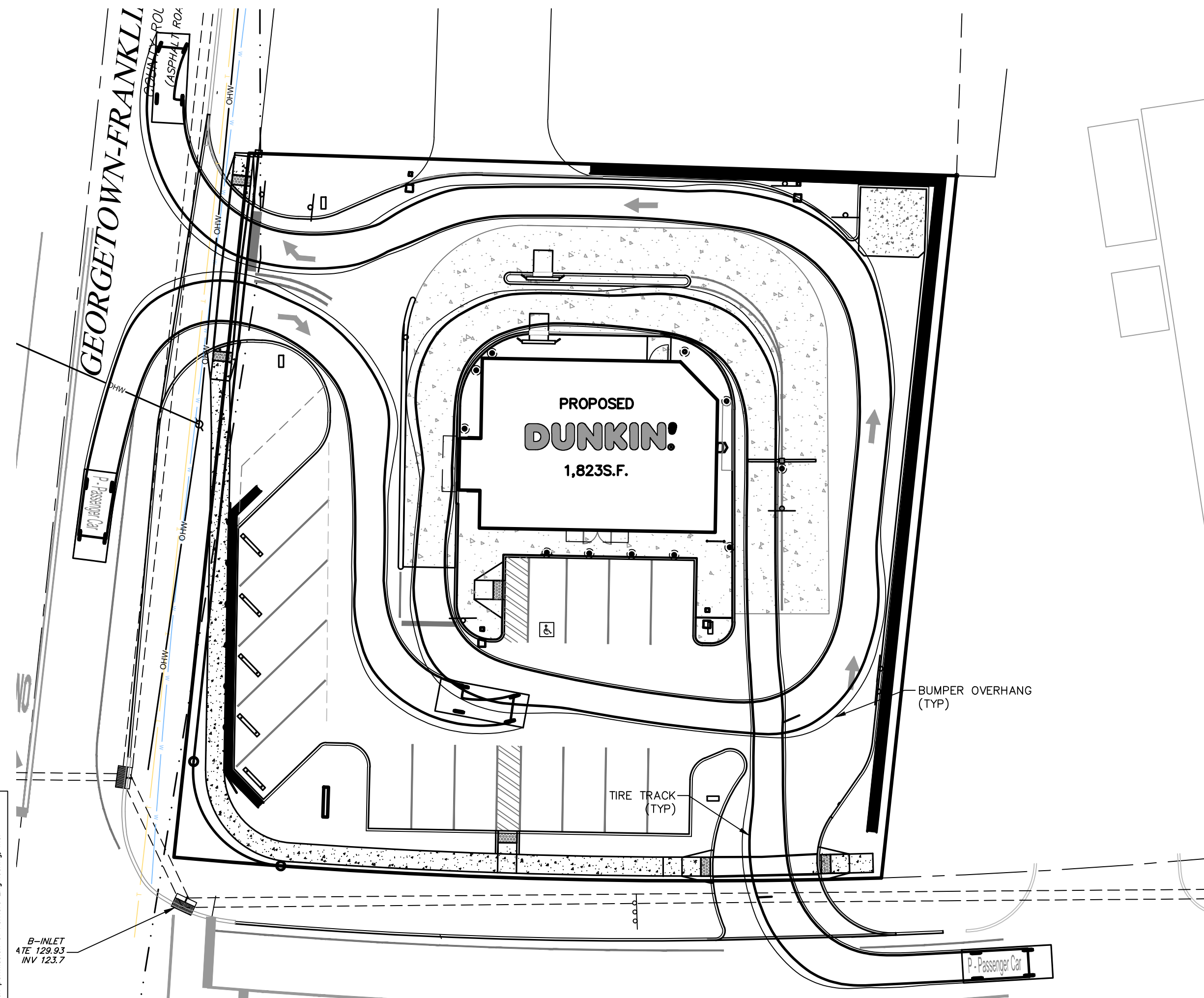
**DRIVE-UP WIDOW STACKING PLAN**  
SCALE: 1" = 20'



**WB-40 SEMI TRAILER TRUCK**  
SCALE: 1" = 20'

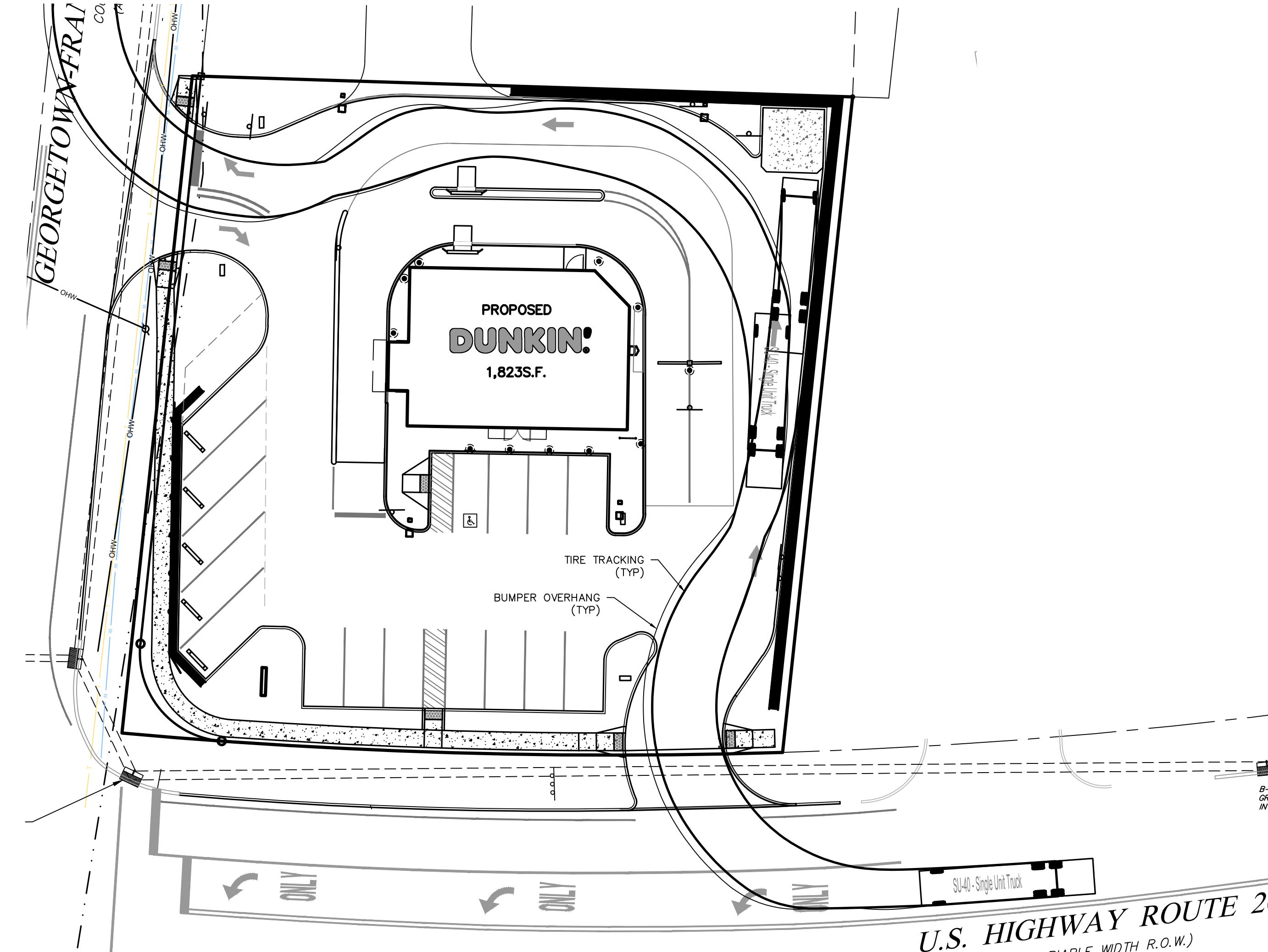
**U.S. HIGHWAY ROUTE 206**  
(VARIABLE WIDTH R.O.W.)  
(ASPHALT ROADWAY)

- LEGEND**
- 540 EXISTING CONTOUR LINE
  - 520 PROPOSED CONTOUR LINE
  - 516.3 EXISTING SPOT GRADE
  - + 640.00 PROPOSED SPOT GRADE
  - EXISTING CURB LINE
  - PROPOSED CURB LINE
  - EXISTING INLET
  - PROPOSED INLET
  - EXISTING STORM/SAN PIPE
  - PROPOSED STORM SEWER
  - EXISTING SITE LIGHT
  - PROPOSED SITE LIGHT
  - G EXISTING GAS LINE
  - W EXISTING WATER LINE
  - EXISTING OVERHEAD WIRE
  - EXISTING UTILITY POLE
  - EXISTING STONE WALL
  - EXISTING FENCE
  - PROPOSED CONCRETE
  - EXISTING SITE LIGHT



**PASSENGER VEHICLE TURNING PLAN**  
SCALE: 1" = 20'

**U.S. HIGHW.**  
(VARIABLE WIDTH R.O.W.)  
(ASPHALT ROADWAY)

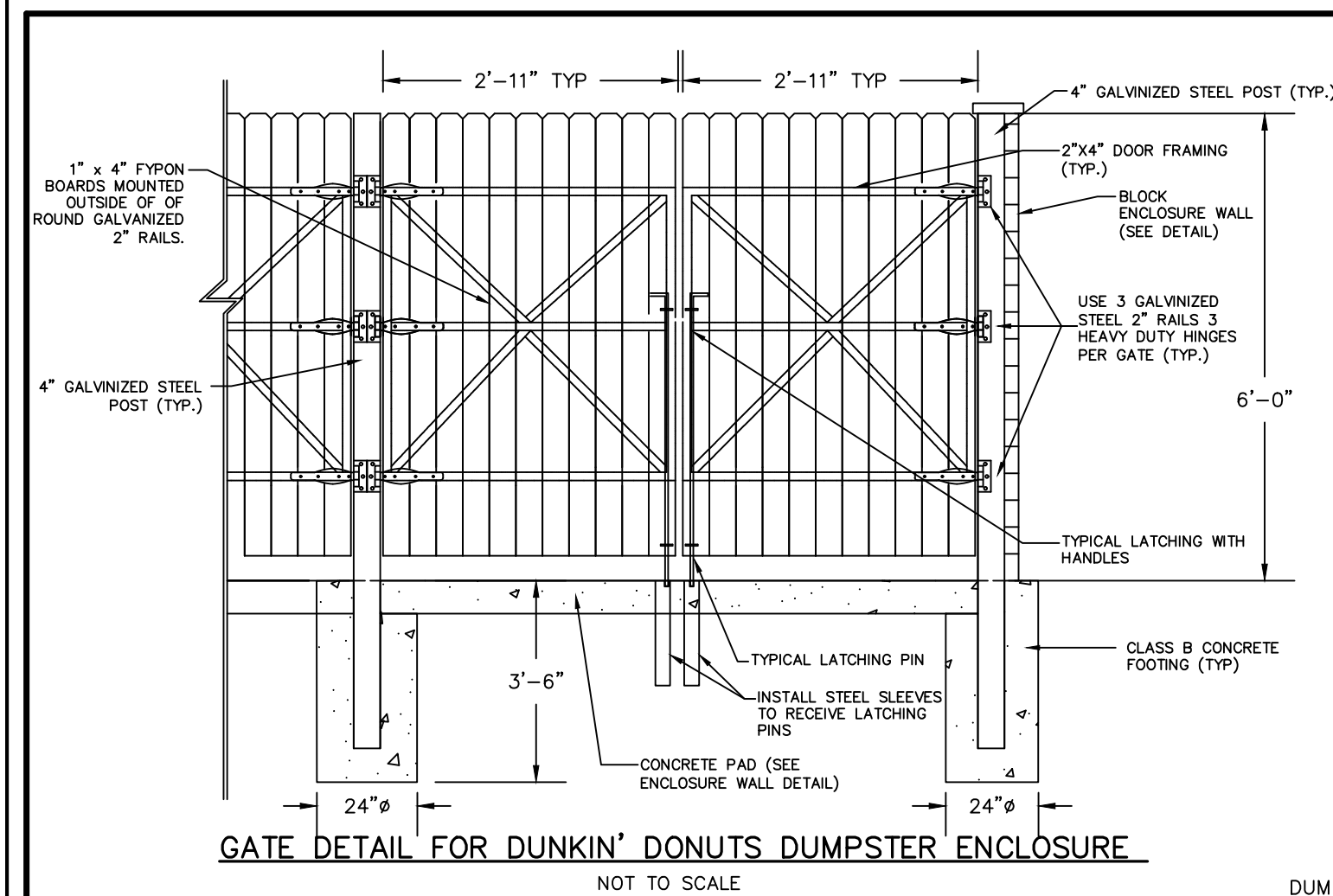


**SU-40 REFUSE COLLECTION TRUCK**  
SCALE: 1" = 20'

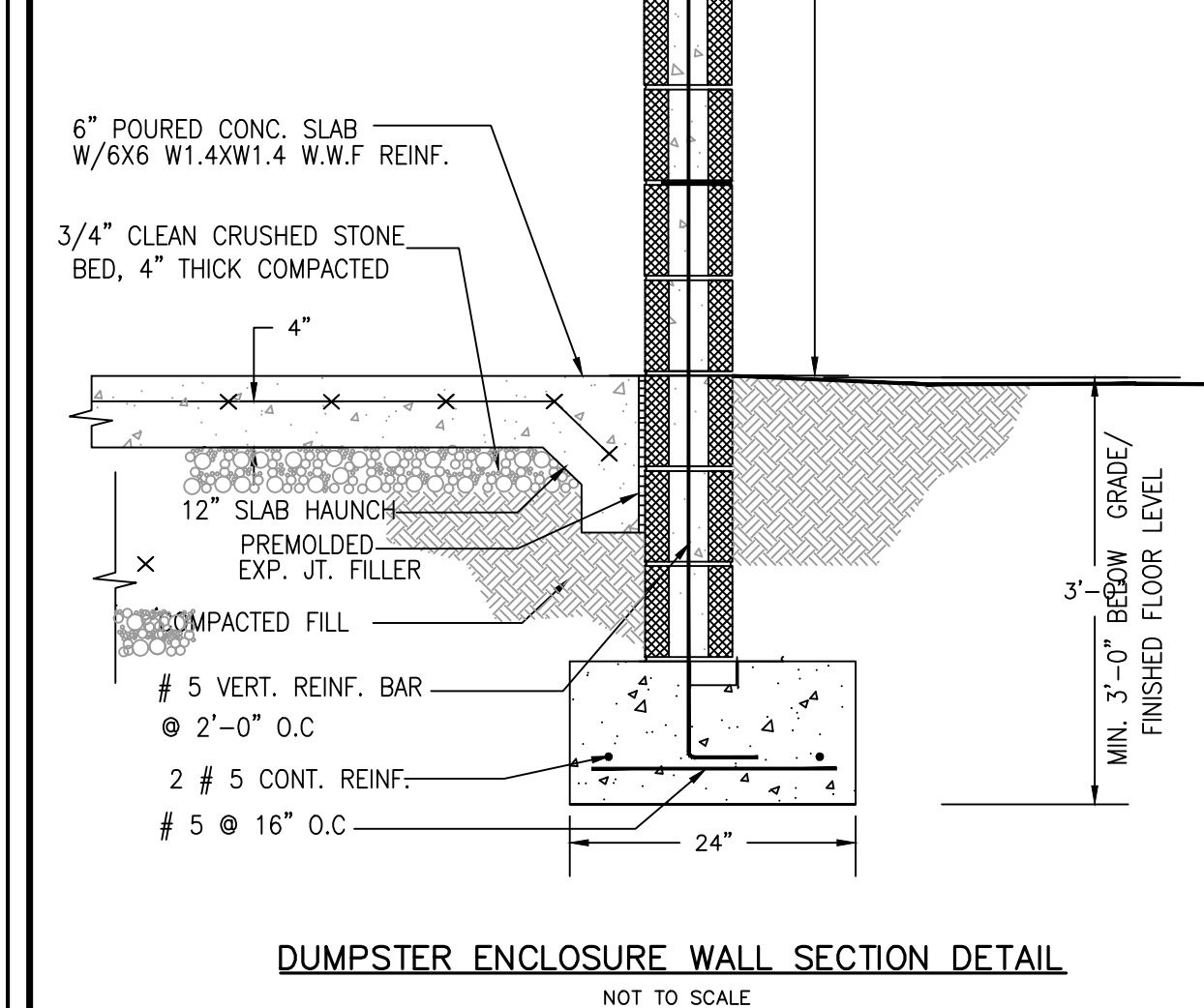
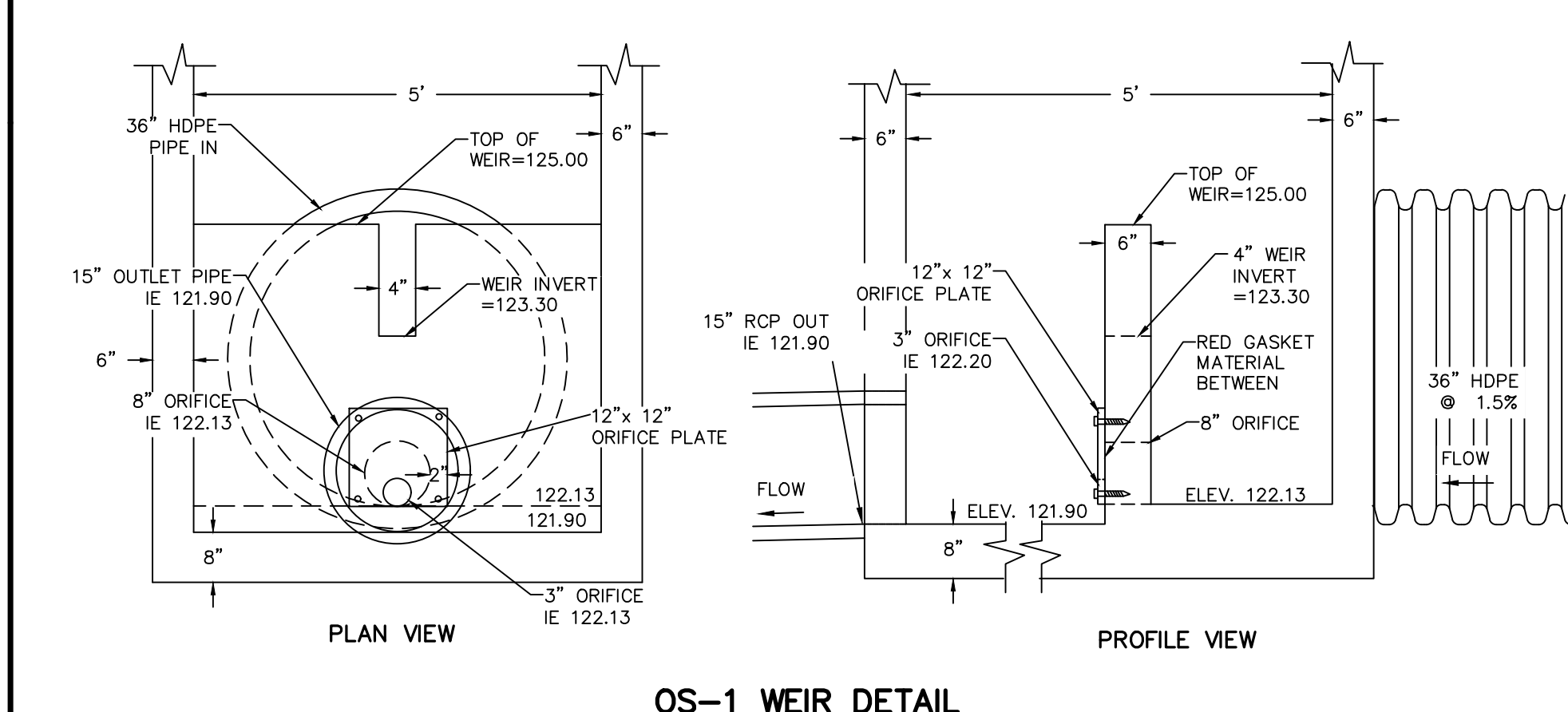
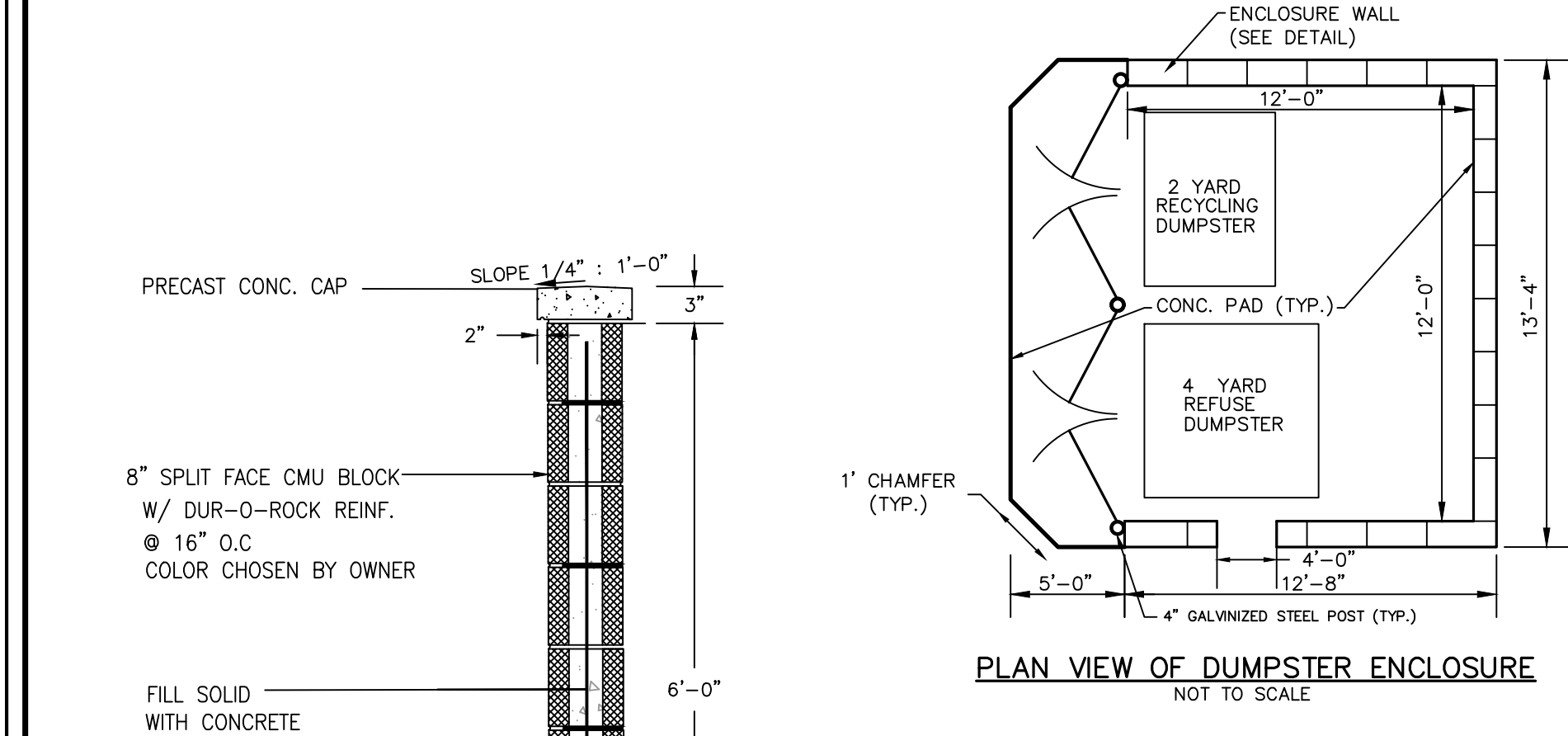
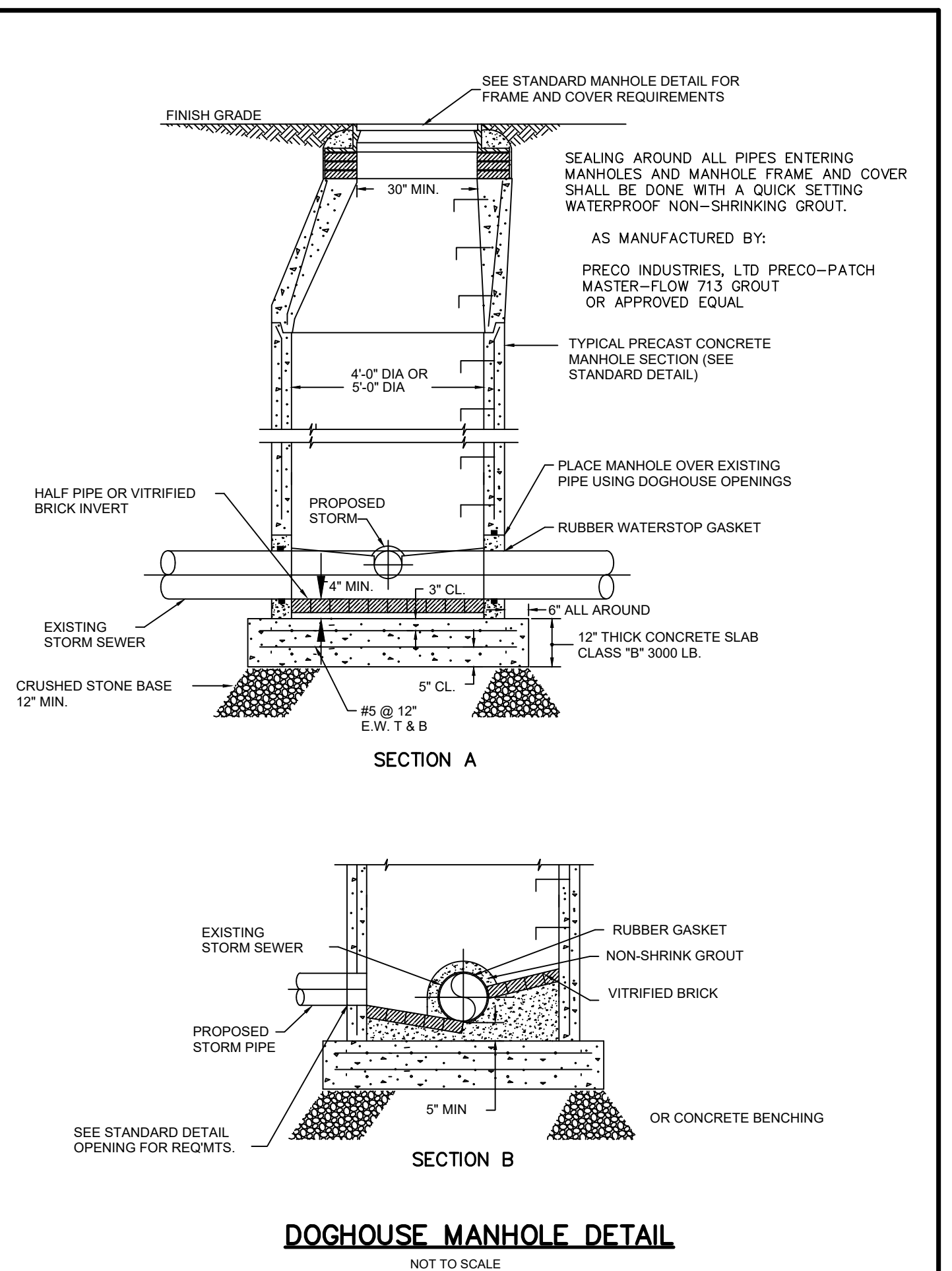
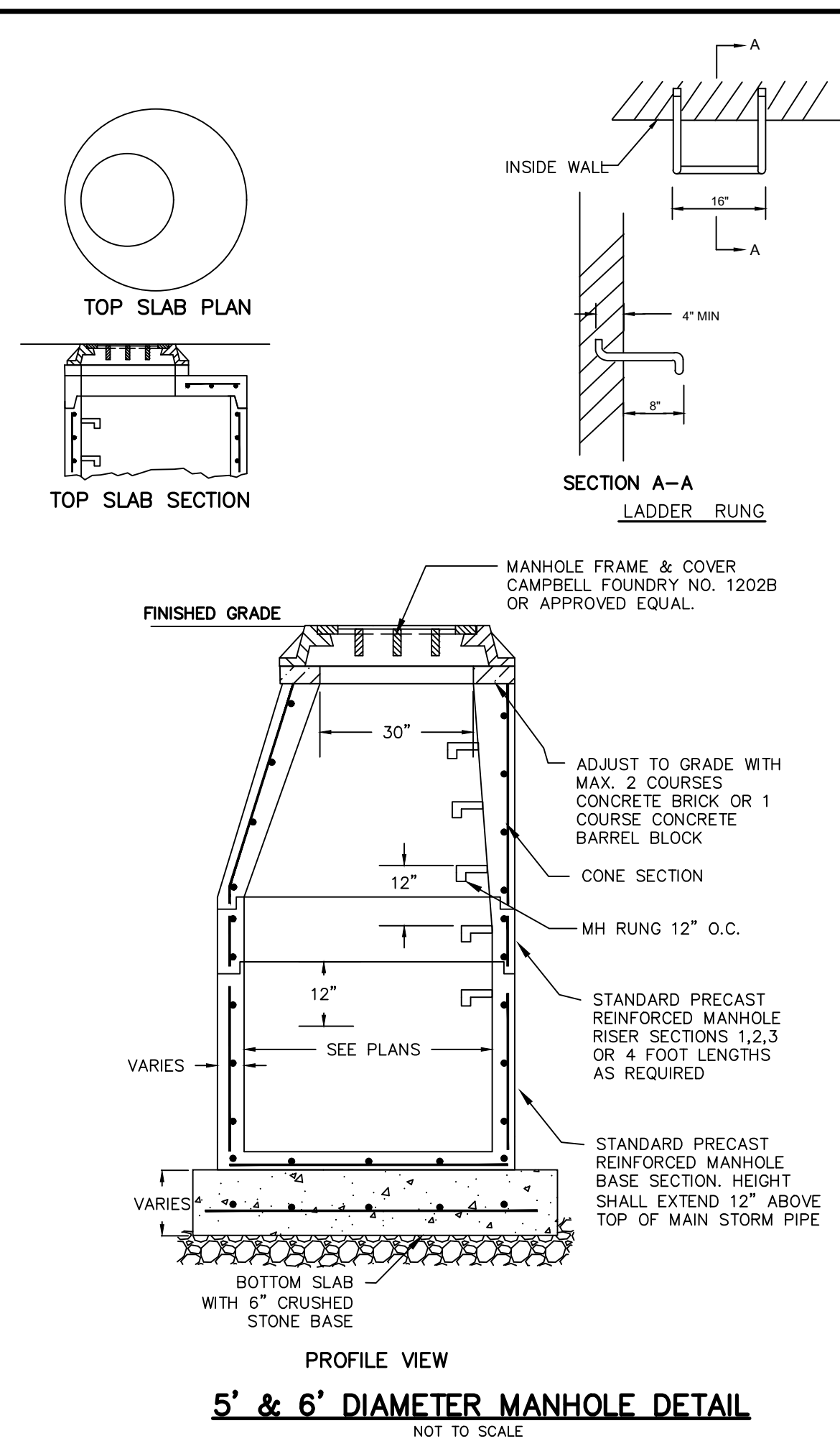
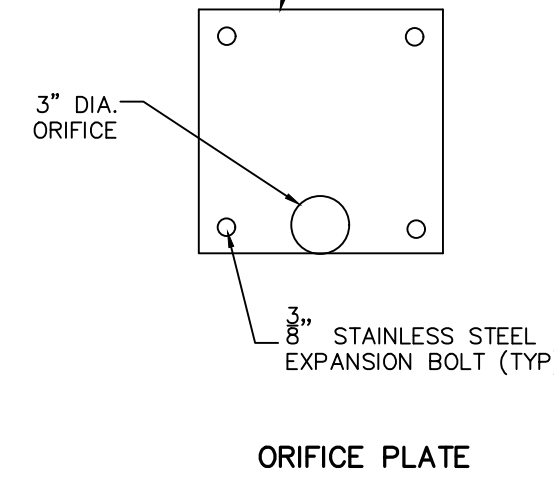
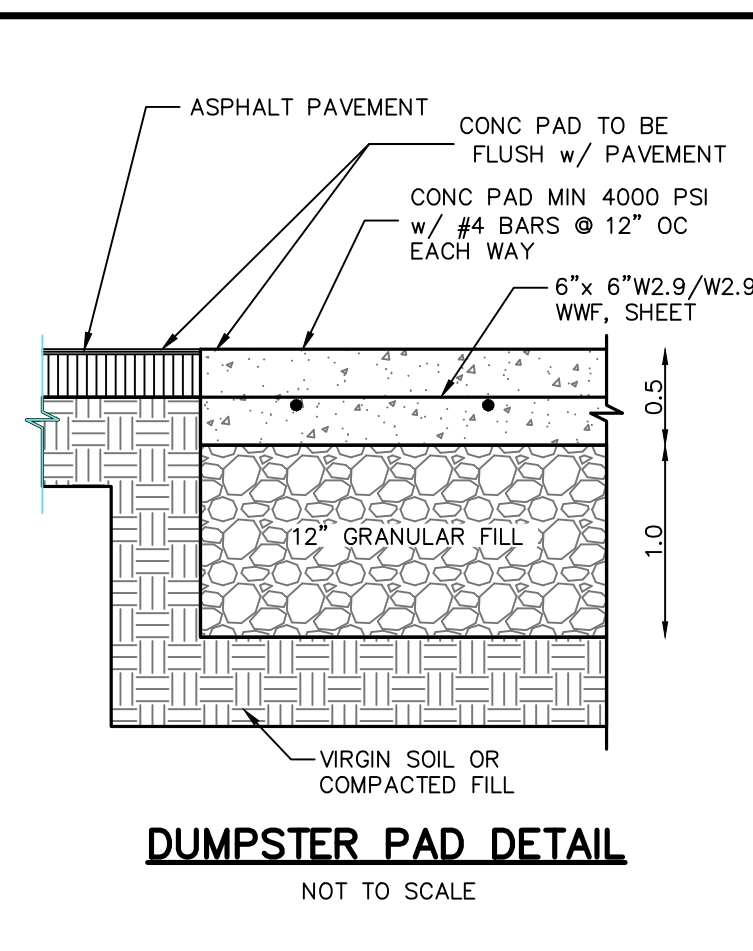
**U.S. HIGHWAY ROUTE 206**  
(VARIABLE WIDTH R.O.W.)  
(ASPHALT ROADWAY)

1 5/20/2021 PER TWP COMPLETENESS REVIEW	
NO.	REVISION
DRAWN BY: SCALE: 1" = 20' FIELD BOOK:	
AMH	
CHKD BY: FILE:	
PWF	41046/DWG/BASEMAPS/BASEMAPS-9-2020
PAUL W. FERRIERO N.J. PROFESSIONAL ENGINEER NO. <u>GE32978</u>	
 <b>Ferriero Engineering, Inc.</b> 180 MAIN STREET P.O. BOX 571 CHESTER, NEW JERSEY 07930 908-879-6209 CERTIFICATE OF AUTHORIZATION 24CA27935400	
<b>CIRCULATION PLAN</b>	
<b>DUNKIN'!</b>	
LOT 64 BLOCK 28005 MONTGOMERY TOWNSHIP SOMERSET COUNTY NEW JERSEY	
SHEET 10 OF 13	
DATE: 2/25/2021	PROJECT NO: 141046
REVISION:	

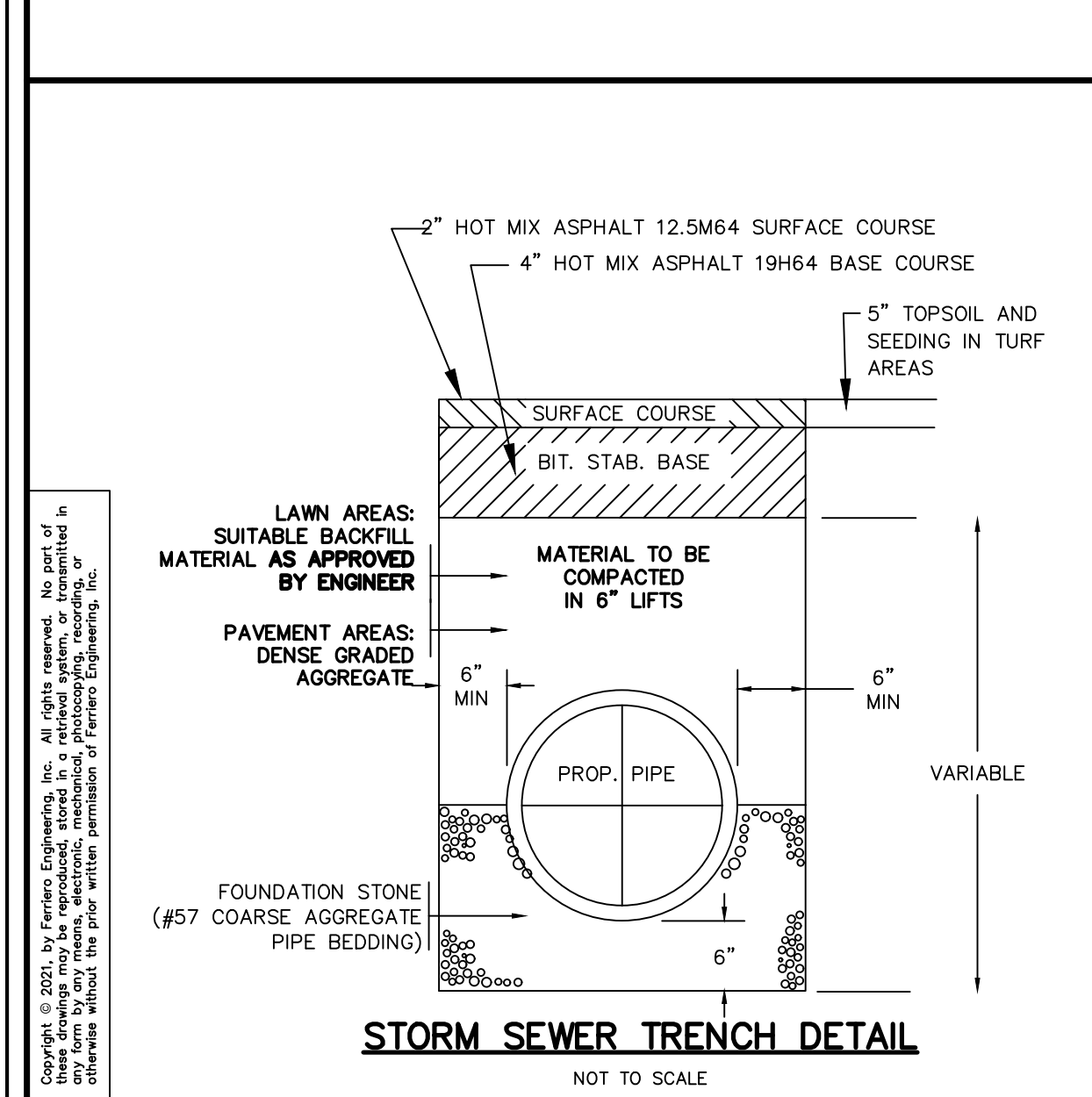




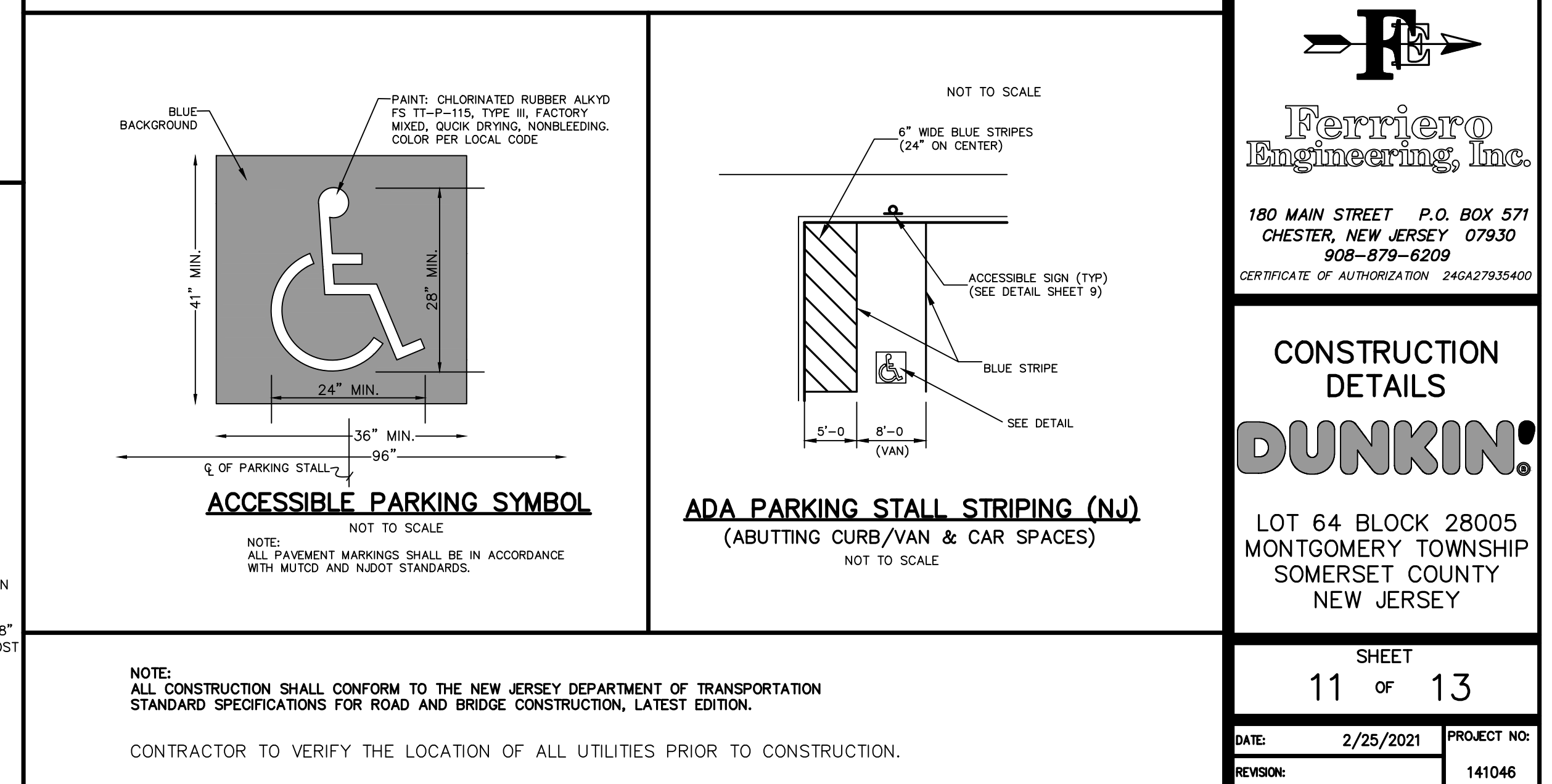
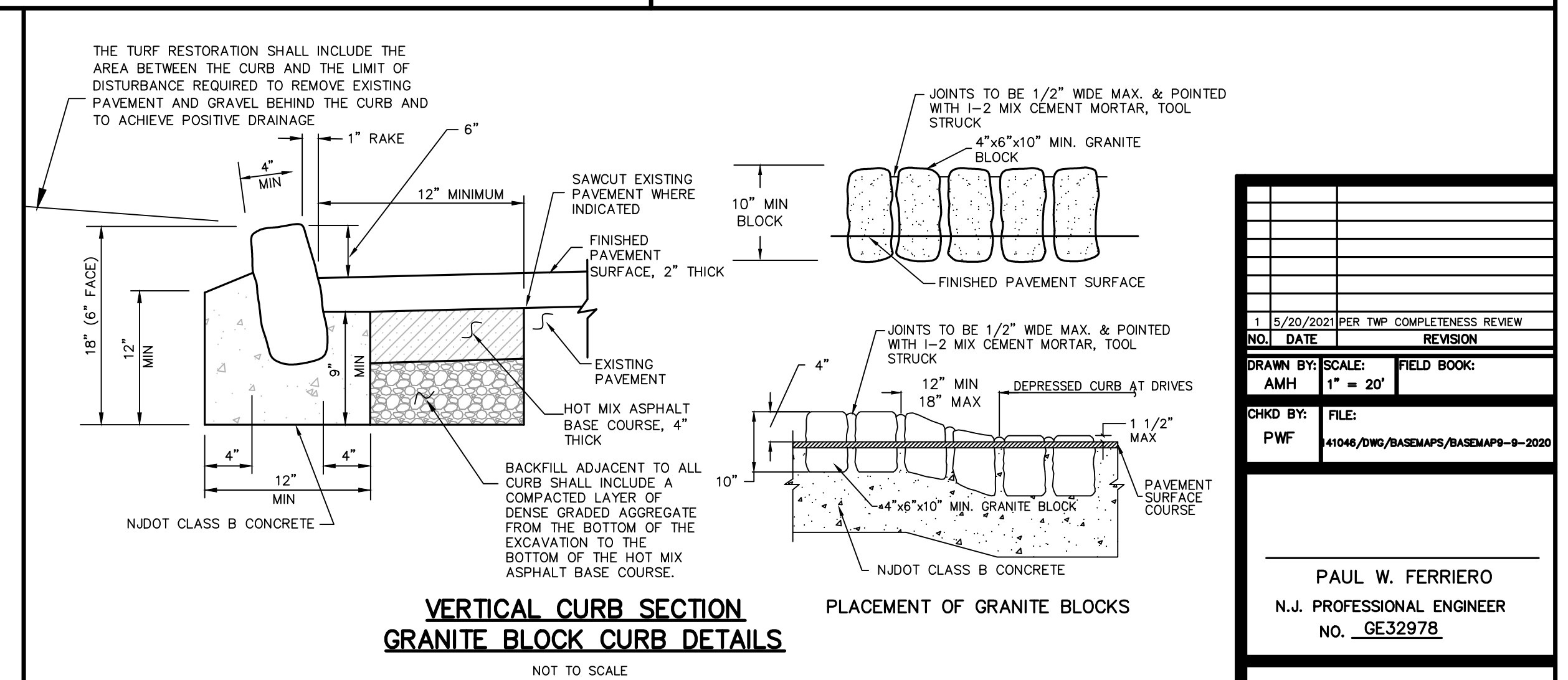
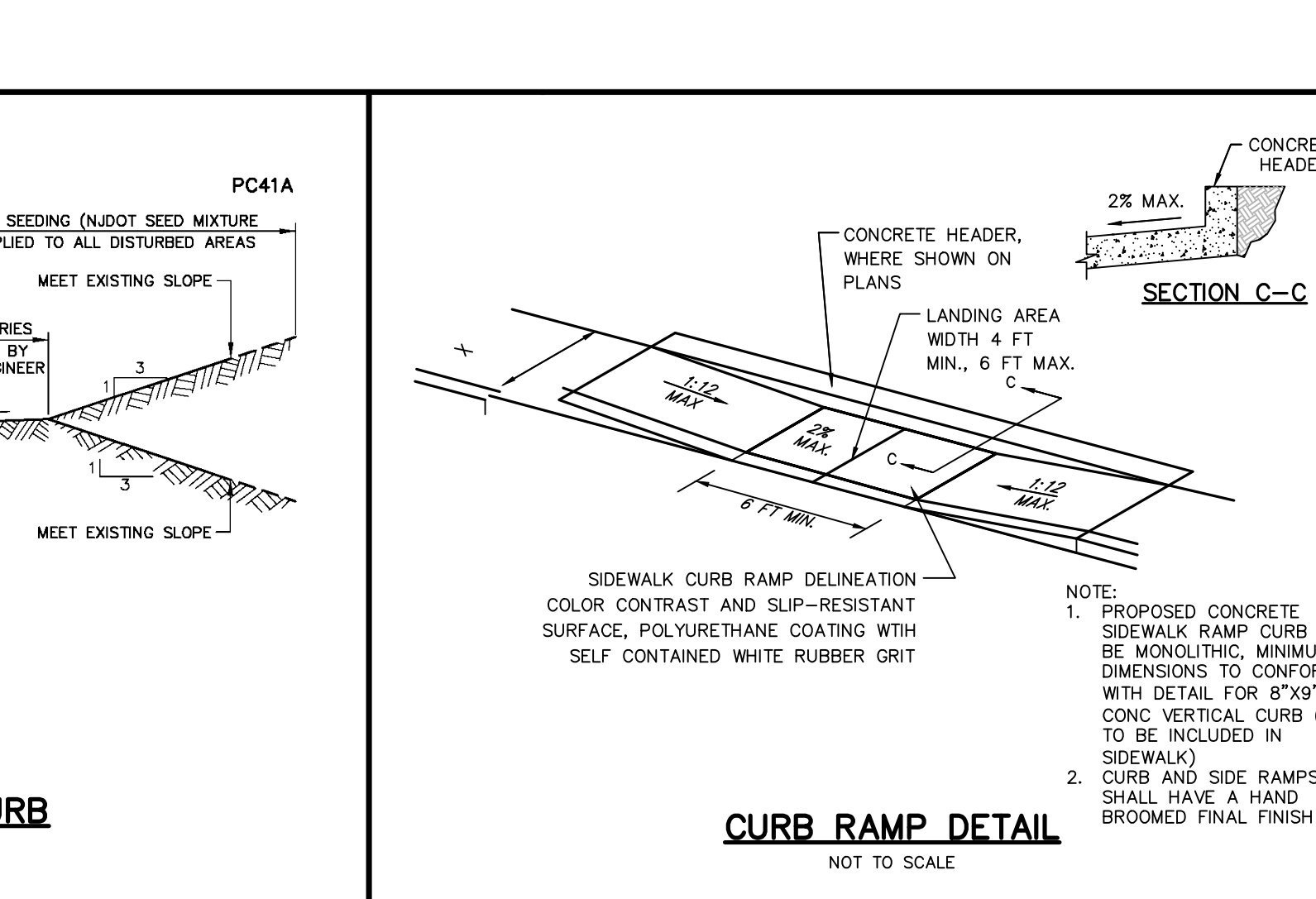
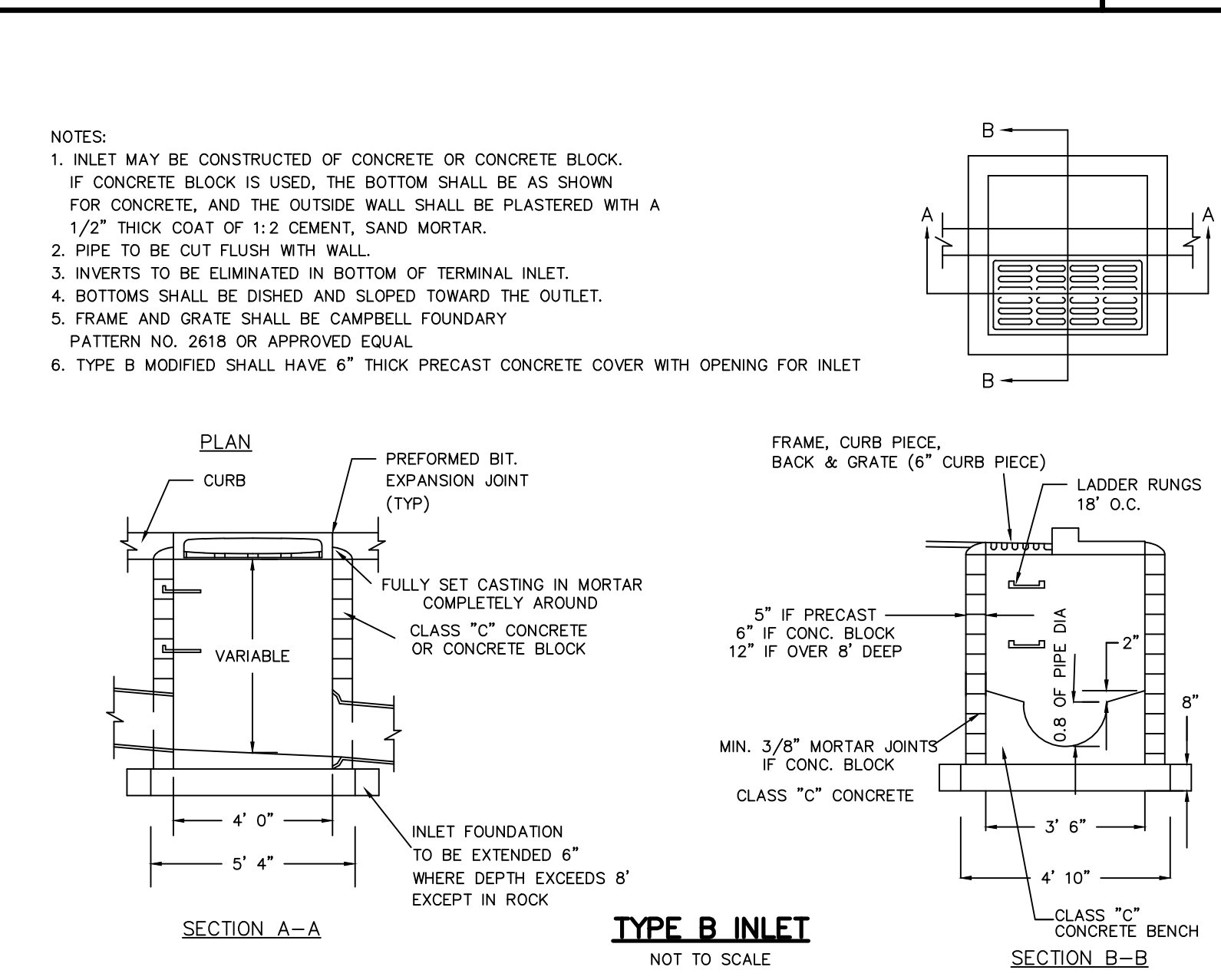
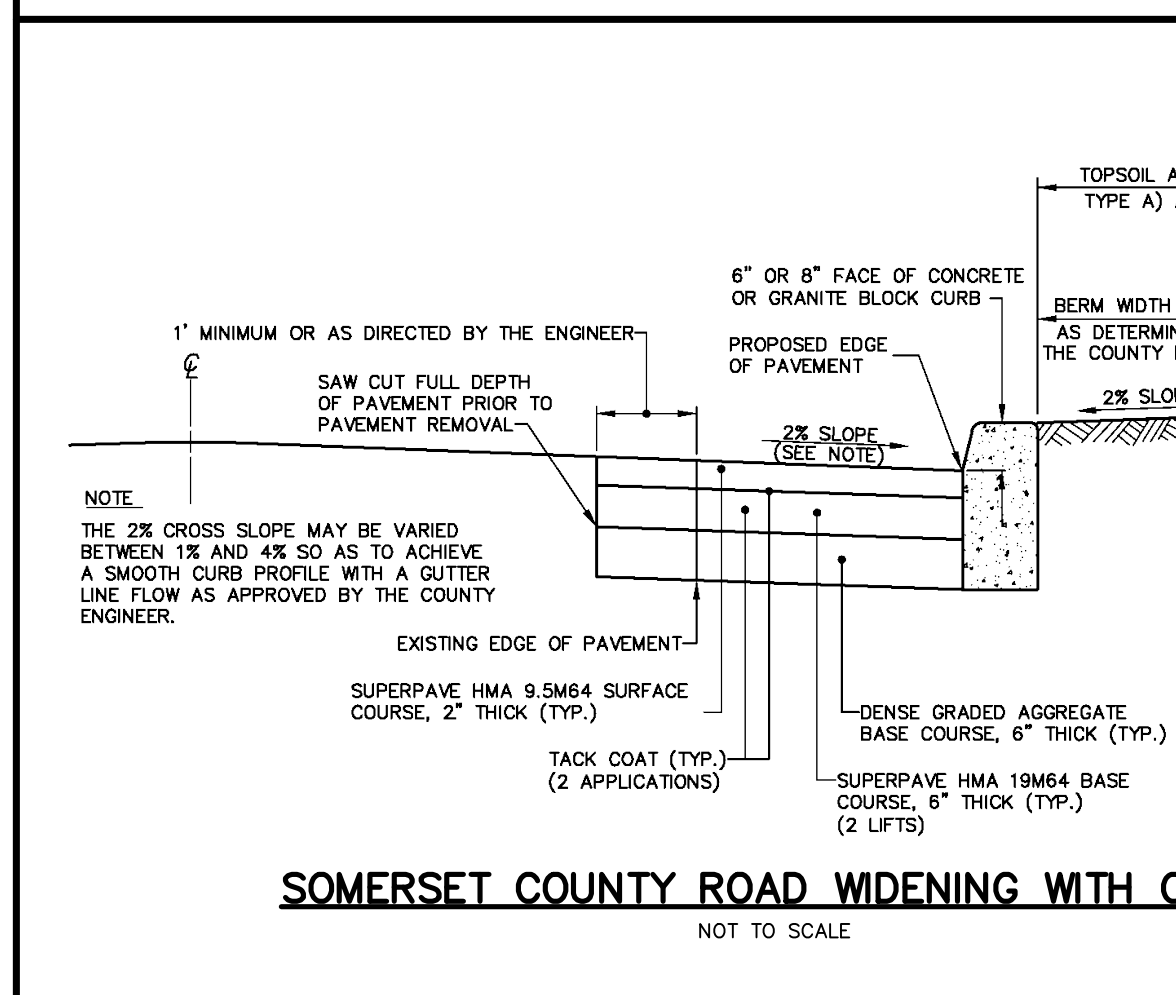
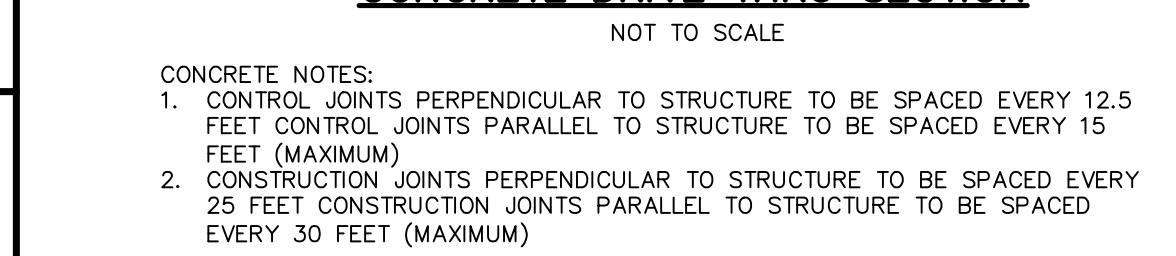
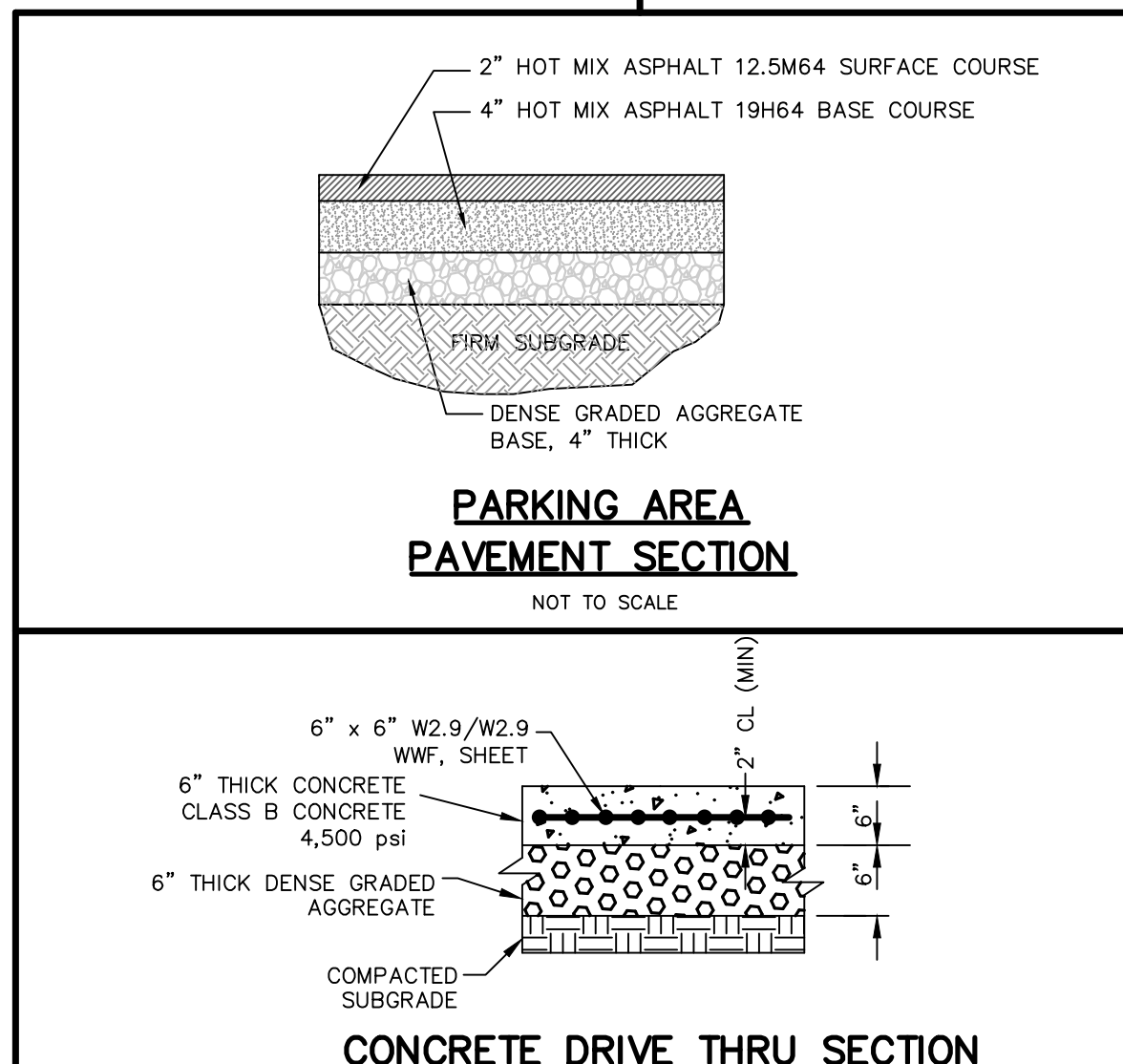
- NOTES:
1. ALL POSTS, RAILS AND FITTINGS TO BE GALVANIZED
  2. ALL WIRE, POSTS & FITTINGS SHALL BE HOT-DIP GALVANIZED STEEL
  3. GATE TO BE CONSTRUCTED OF FYPION LUMBER.
  4. PAD TO BE 3,500 P.S.I. CONCRETE
  5. SEPARATE DUMPSTERS SHALL BE CLEARLY MARKED REFUSE AND RECYCLING.
  6. BLOCK TO BE PAINTED TO MATCH PROPOSED BUILDING COLOR AND SEALED.



**DUMPSTER ENCLOSURE WALL SECTION DETAIL**  
NOT TO SCALE



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1. 5/20/2021 PER TWP COMPLETENESS REVIEW	
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PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978

**Ferriero Engineering, Inc.**

180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24GA27935400

**CONSTRUCTION DETAILS**

**DUNKIN'**

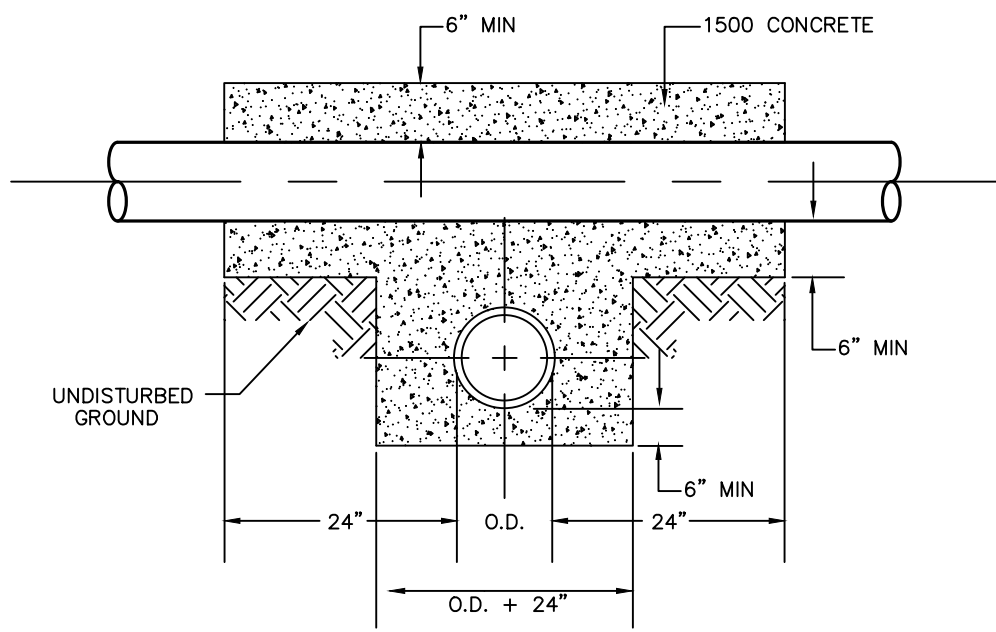
LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET 11 OF 13

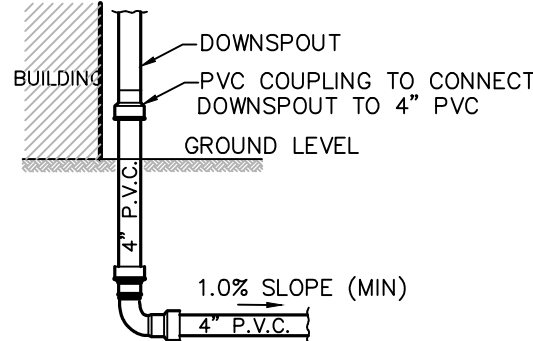
DATE: 2/25/2021 PROJECT NO: 141046



- NOTES:
- ENCASEMENT OF LOWER PIPE TO BE CARRIED IN 24" IN EACH DIRECTION BEYOND THE WALLS OF THE UPPER CONDUIT.
  - THE ENCASEMENT SHOWN SHALL BE REQUIRED WHERE THE CLEARANCE BETWEEN PIPES IS 12" OR LESS.
  - WATER PIPE SHALL BE PLACED ABOVE OTHER UTILITIES UNLESS OTHERWISE DIRECTED BY THE WATER UTILITY.



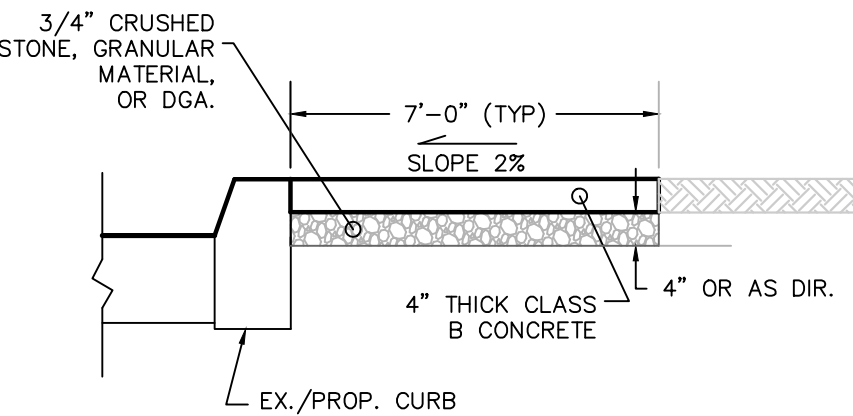
ELEVATION  
**UTILITY CROSSING**  
NOT TO SCALE



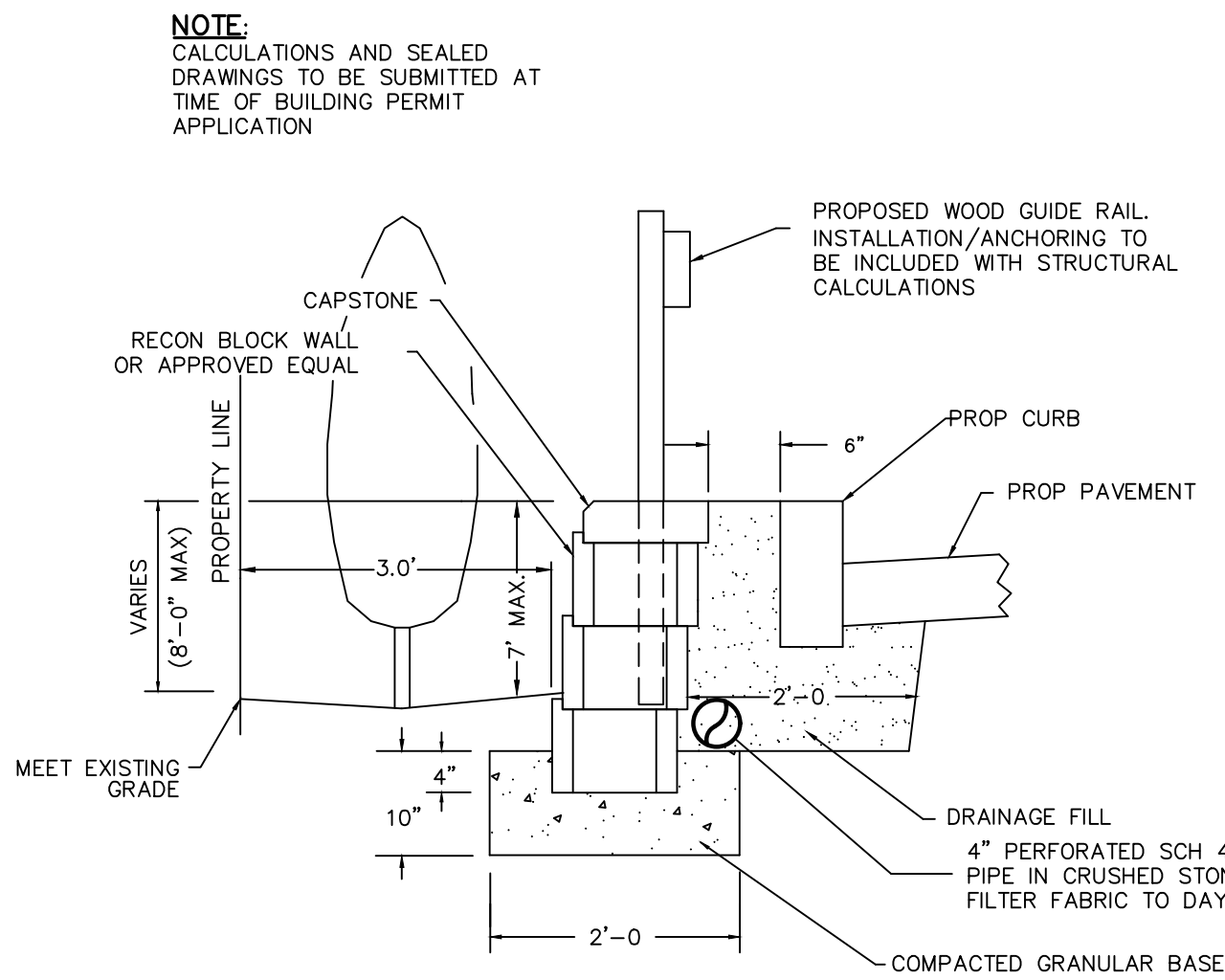
**DOWNSPOUT/LEADER CONNECTION**

NOTE: ALL LEADER DRAINS TO HAVE 1'-0" COVER (MIN)

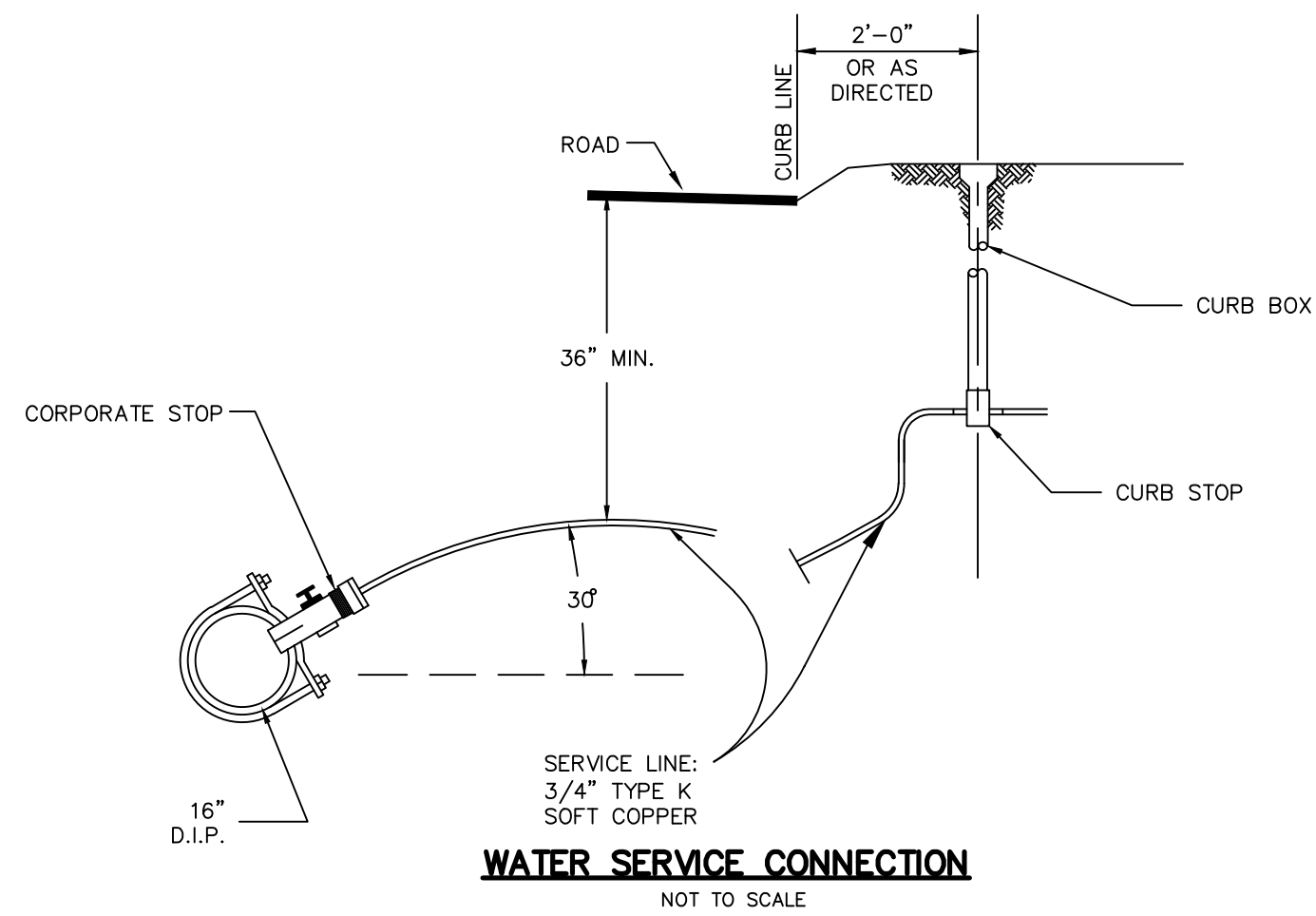
NOT TO SCALE



**CONCRETE SIDEWALK**  
NOT TO SCALE

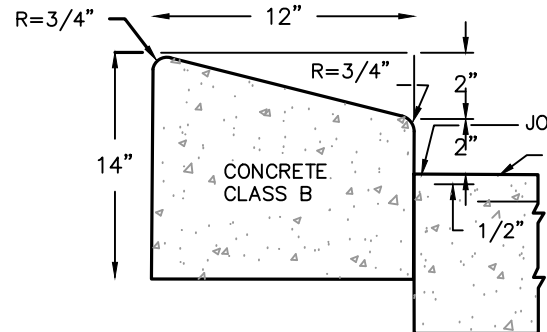


**ENGINEERED RETAINING WALL SECTION**  
NOT TO SCALE

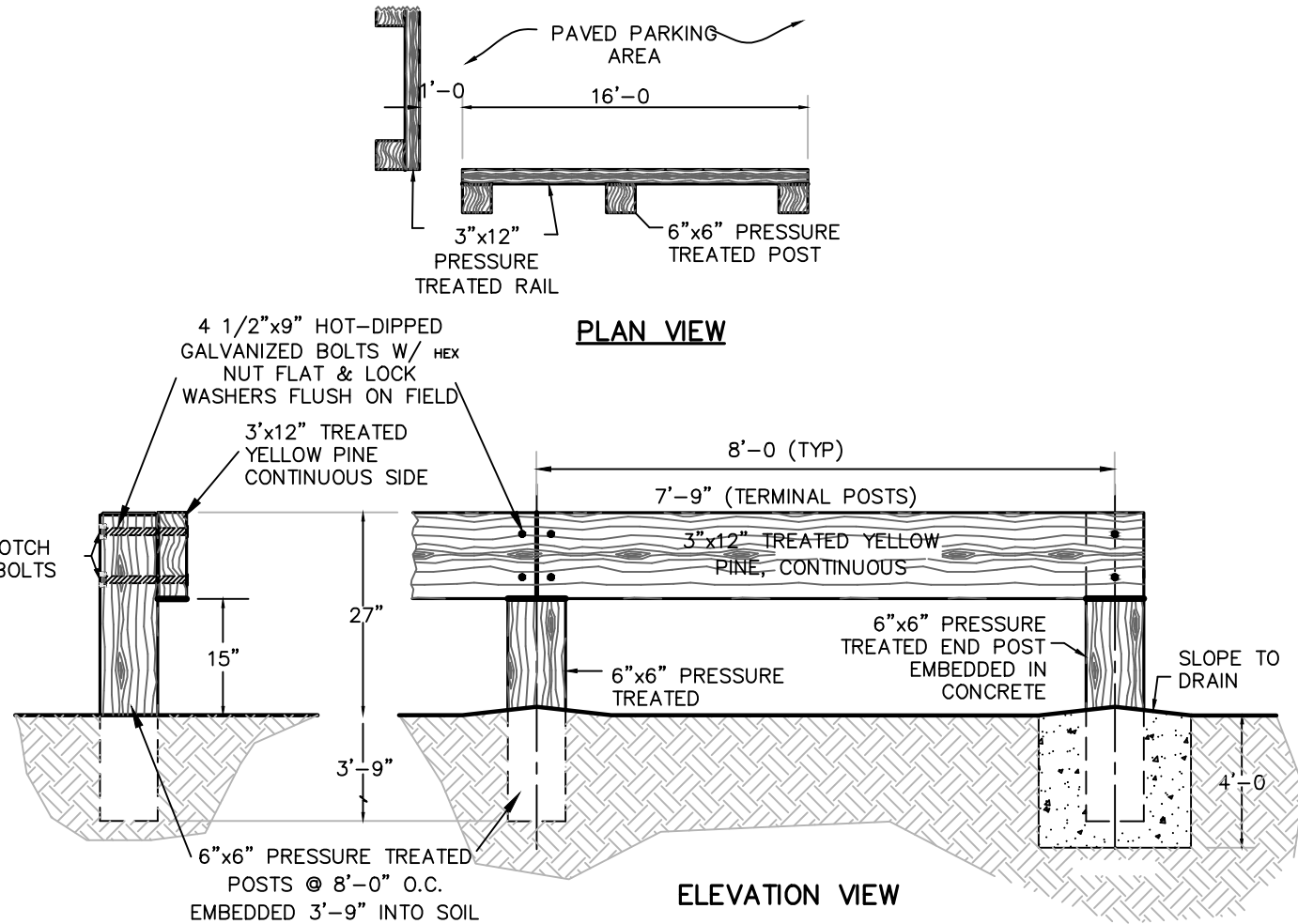


**WATER SERVICE CONNECTION**  
NOT TO SCALE

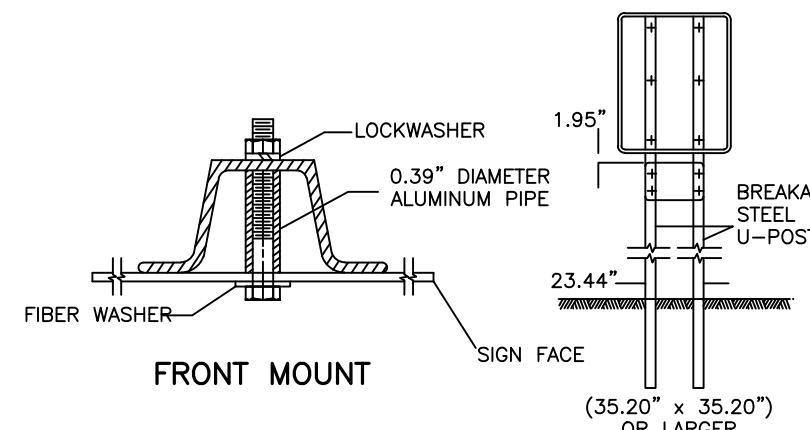
2" PREFORMED BITUMINOUS JOINT FILLER  
DEPTH OF FILLER STRIP EQUALS PAVEMENT THICKNESS  
LESS 1/2" TO BE INSTALLED BETWEEN CURB AND CONCRETE PAVEMENT OR CONCRETE BASE COURSE.



**12"x 13" CONCRETE SLOPING CURB**  
NOT TO SCALE

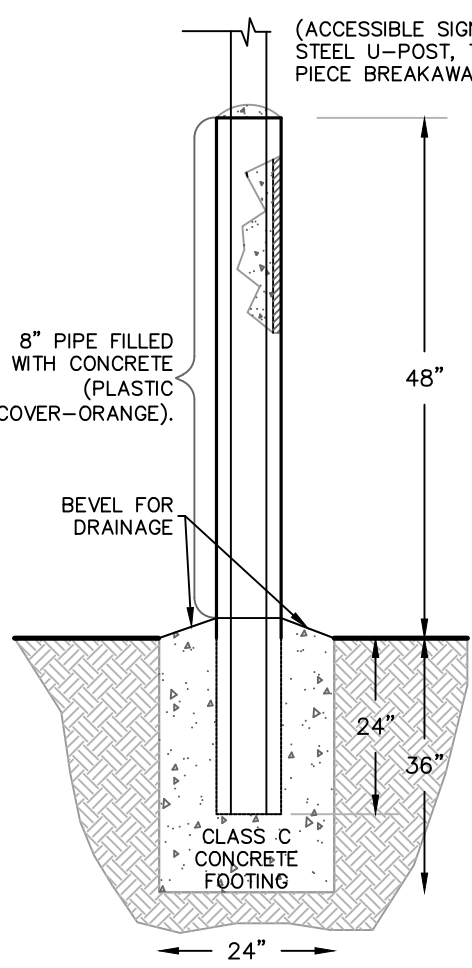


SECTION VIEW  
**WOOD GUIDE RAIL**  
NOT TO SCALE

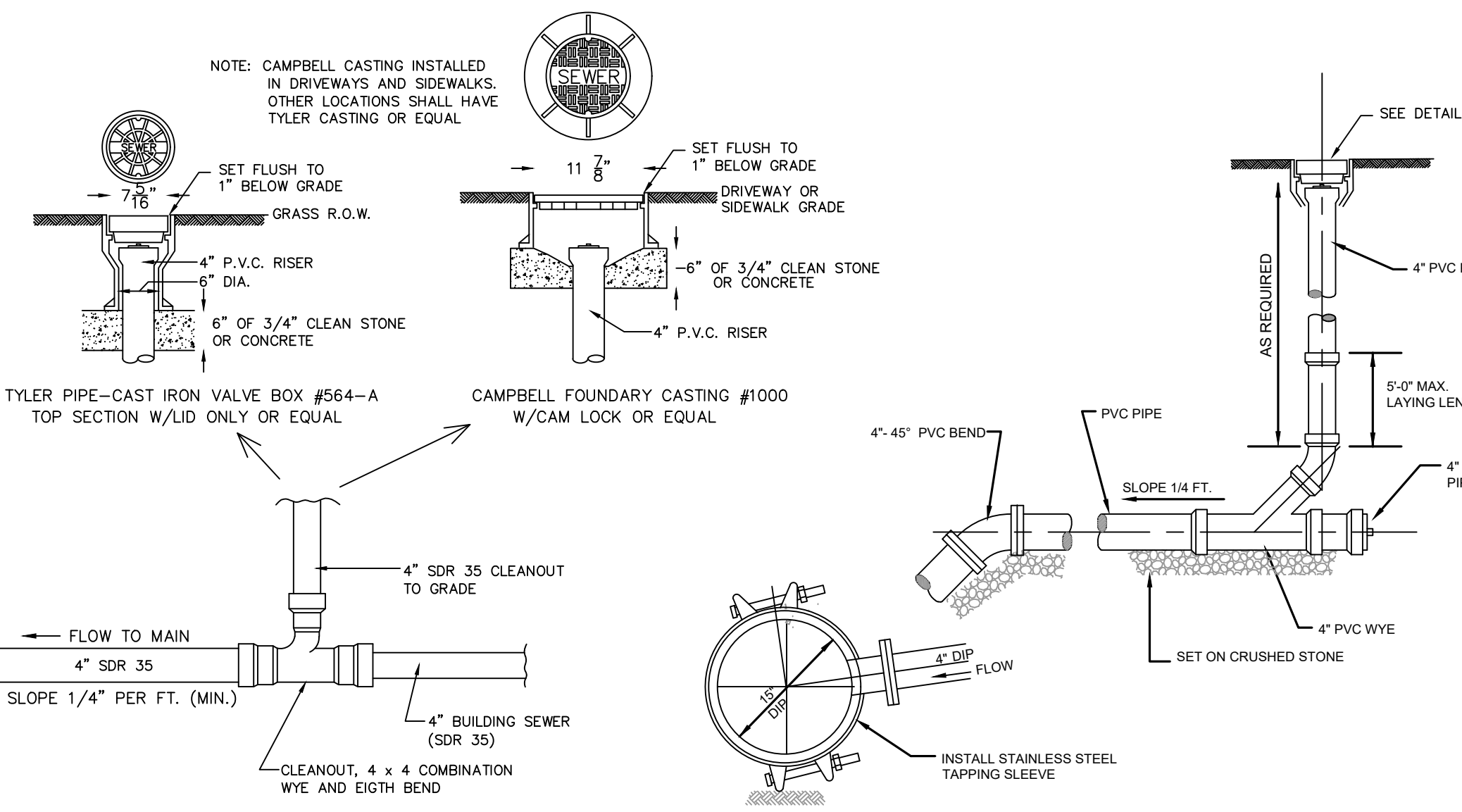


**SIGN POST EXTENSION  
SPLICE DETAILS**  
NOT TO SCALE

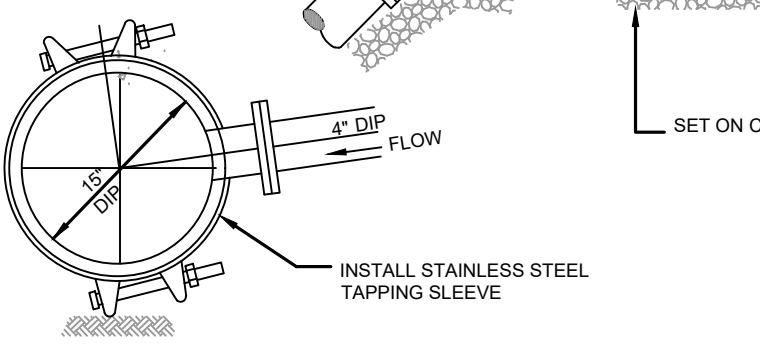
**SIGN NOTES:**  
ALL SIGN WORK--SIZE, COLOR, CONTENT, EMBEDMENT IN GROUND ETC, SHALL BE AS PER N.J.D.O.T. STANDARDS AND THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).



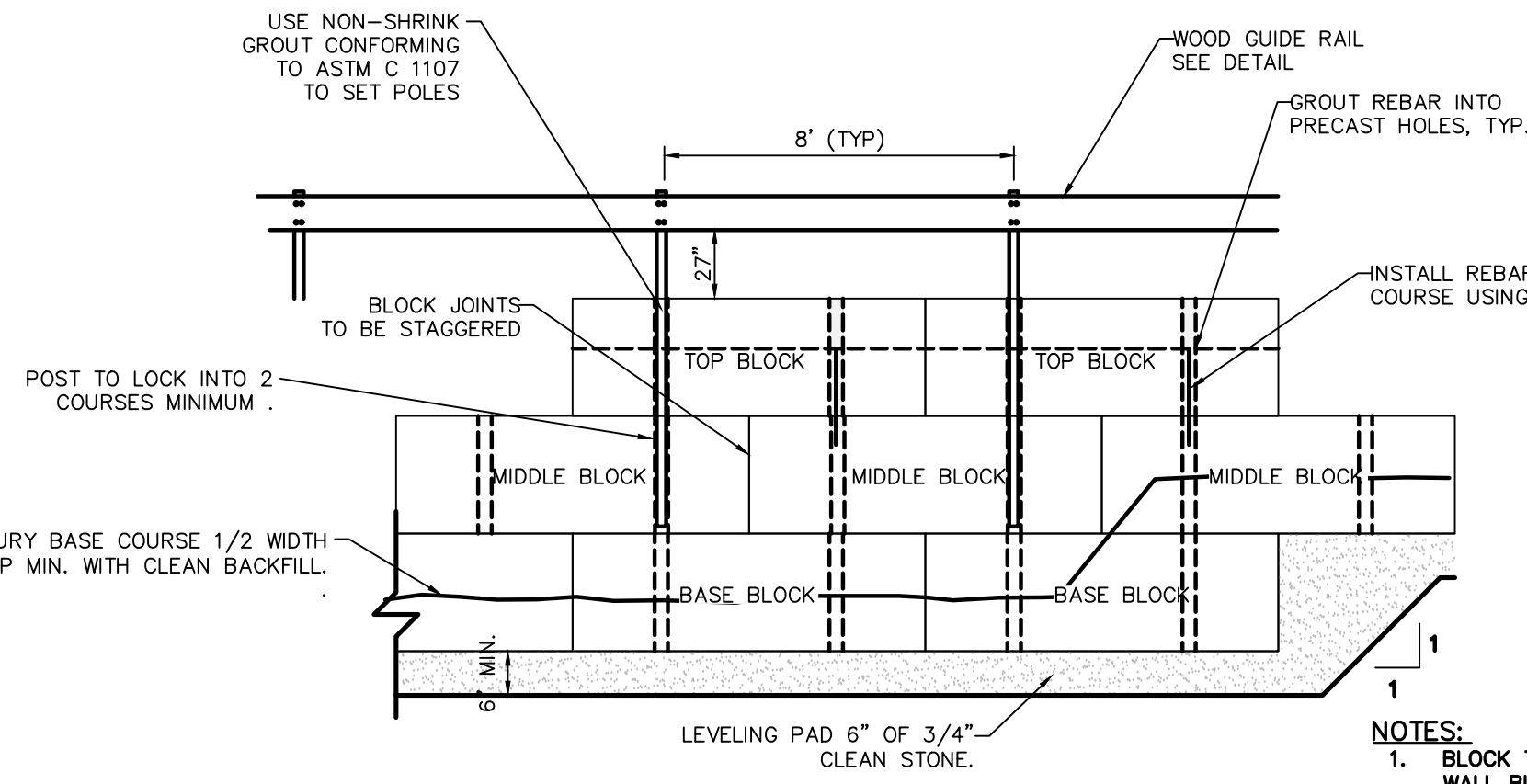
**BOLLARD DETAIL**  
NOT TO SCALE



**CLEANOUT DETAIL**  
NOT TO SCALE

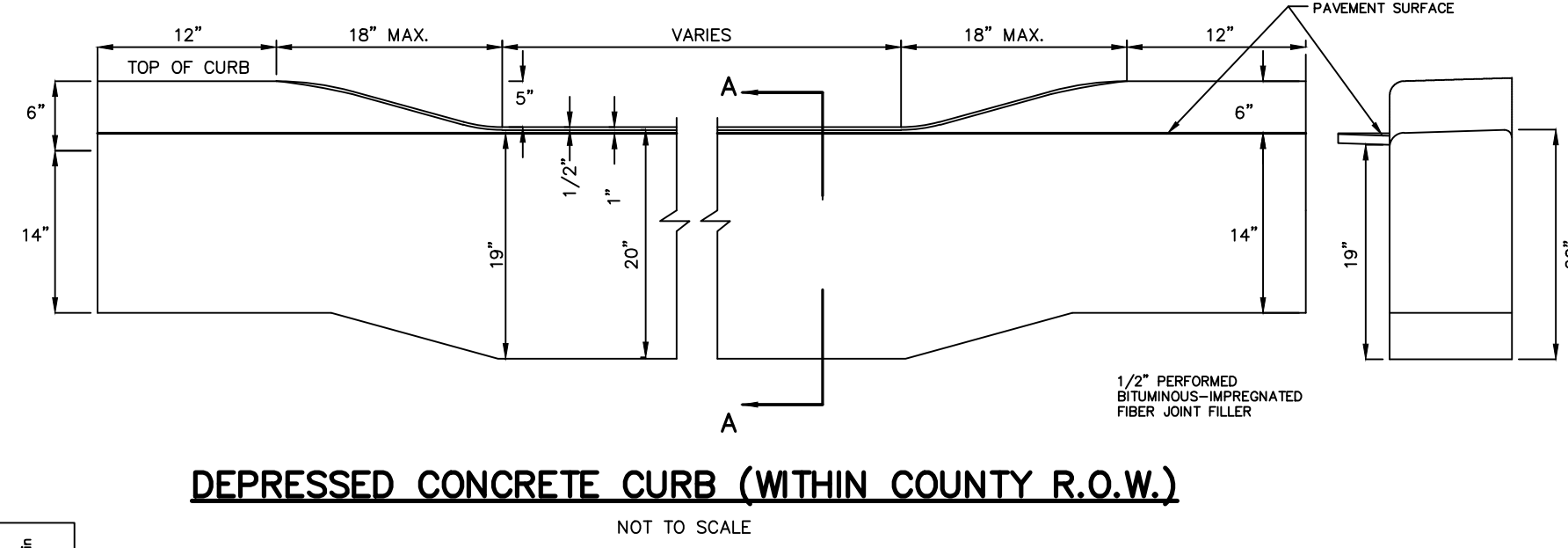


**SANITARY CONNECTION DETAIL**  
NOT TO SCALE

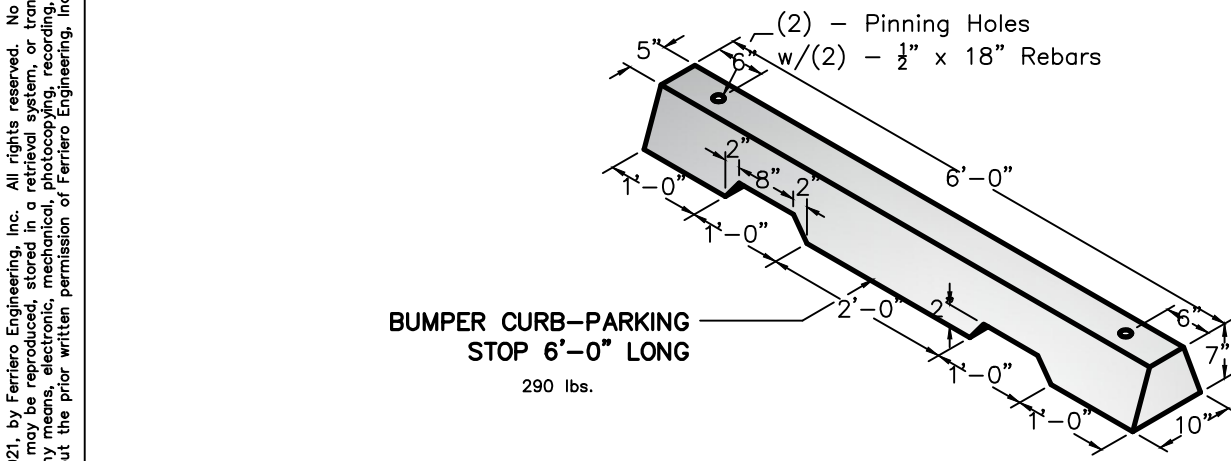


**WOOD GUIDE RAIL INSTALLATION DETAIL**  
NOT TO SCALE

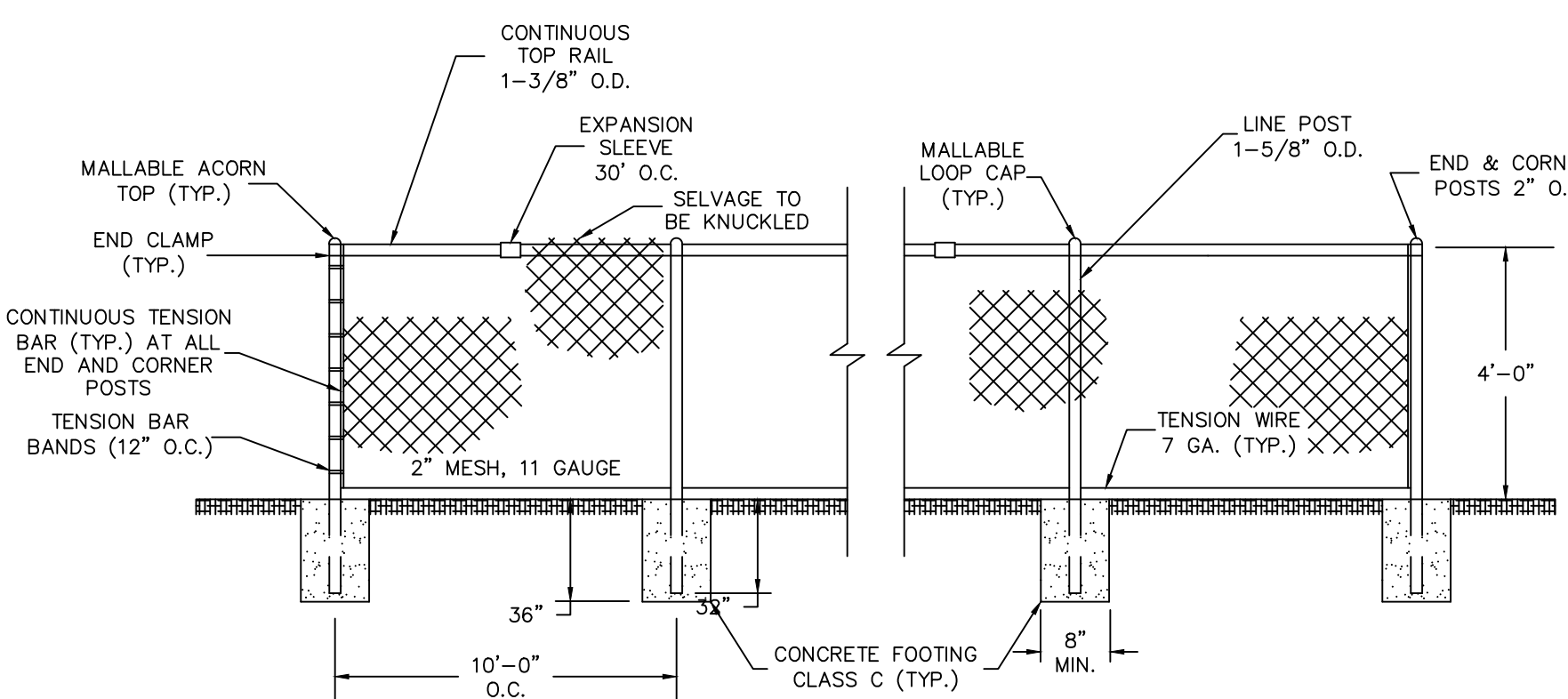
- NOTES:
- BLOCK TO BE RECON GRAVITY RETAINING WALL BLOCK OR APPROVED EQUAL.
  - WHERE GUIDE RAIL TO BE INSTALLED, USE FENCE WALL BLOCK.
  - BLOCK TO BE TEXTURED.
  - BLOCK TO BE STAINED A COLOR TO MATCH THE PROPOSED BUILDING.
  - BLOCK TO BE SEALED AFTER STAINING.



**DEPRESSED CONCRETE CURB (WITHIN COUNTY R.O.W.)**  
NOT TO SCALE

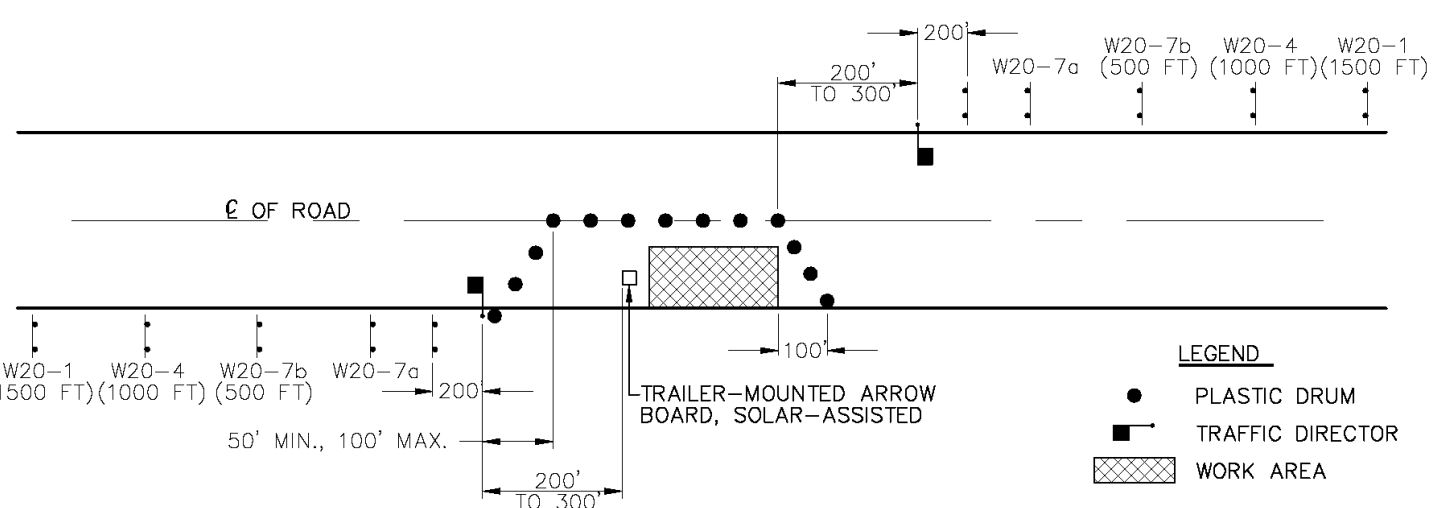


**CURB PARKING BLOCK**  
NOT TO SCALE



- NOTES:
- ALL POSTS, RAILS AND FITTINGS TO BE GALVANIZED AND POWDER COATED TO MATCH THE MESH COLOR.
  - MESH TO BE 1-3/4" OPENING, 9 GAUGE, GALVANIZED.
  - ALL MESH TO BE BLACK VINYL COATED.
  - TIE WIRE TO BE 7 GA ALUM, 12" O.C.

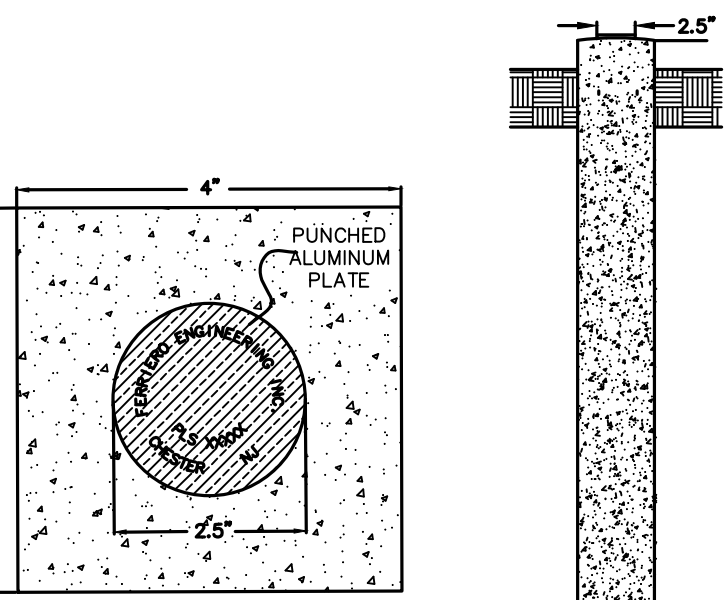
**CHAIN LINK FENCE, 4' HIGH**  
NOT TO SCALE



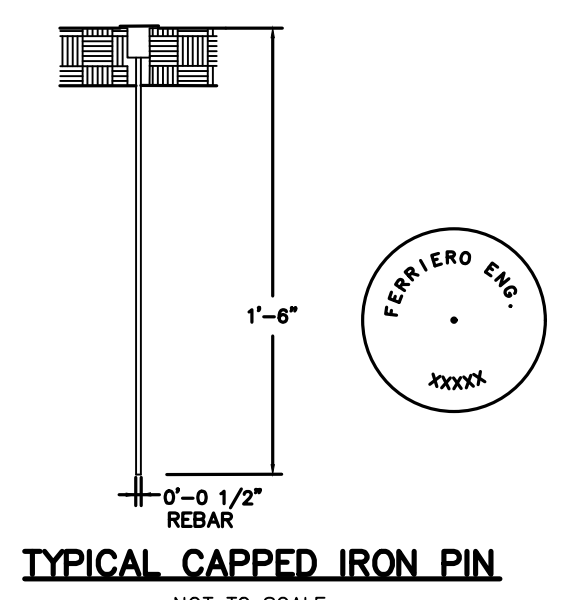
- NOTES:
- ALL SIGNS AND OTHER TRAFFIC CONTROL DEVICES ARE TO CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, WHEREIN THE SIGNS IN THIS FIGURE ARE IDENTIFIED.
  - FLASHING WARNING LIGHTS ARE **NOT** TO BE MOUNTED ON ANY SIGNS.
  - FOR DAYTIME OPERATIONS OF A SHORT DURATION, AND ON ROADS THAT ARE NOT MAJOR ARTERIES, THE W20-1 (AHEAD), W20-7a AND W20-7b SIGNS WILL SUFFICE FOR EACH APPROACH. THE TRAFFIC DIRECTOR AND ALL OTHER DEVICES REMAIN THE SAME.
  - DRUMS ARE TO BE PLACED AT 30' INTERVALS THROUGHOUT THE WORK ZONE.
  - TRAFFIC DIRECTORS SHALL USE STOP/SLOW PADDLES (R1-1) AND, WHERE SIGHT IS OBSTRUCTED, WALKIE-TALKIES.

**TYPICAL ONE-LANE-CLOSURE OPERATION**  
N.T.S.

**TEMPORARY TRAFFIC PLAN**  
NOT TO SCALE



**TYPICAL CONCRETE MONUMENT**  
NOT TO SCALE



**TYPICAL CAPPED IRON PIN**  
NOT TO SCALE

NO.	DATE	REVISION
1	5/20/2021	PER TWP COMPLETENESS REVIEW
2	2/25/2021	REVISION

DRAWN BY: SCALE: FIELD BOOK:  
AMH 1" = 20'

CHKD BY: FILE:  
PWF 41046/DWG/BASEMAPS/BASEMAPS-9-2020

PAUL W. FERRIERO  
N.J. PROFESSIONAL ENGINEER  
NO. GE32978

**Ferriero Engineering, Inc.**

180 MAIN STREET P.O. BOX 571  
CHESTER, NEW JERSEY 07930  
908-879-6209

CERTIFICATE OF AUTHORIZATION 24GA27935400

CONSTRUCTION  
DETAILS

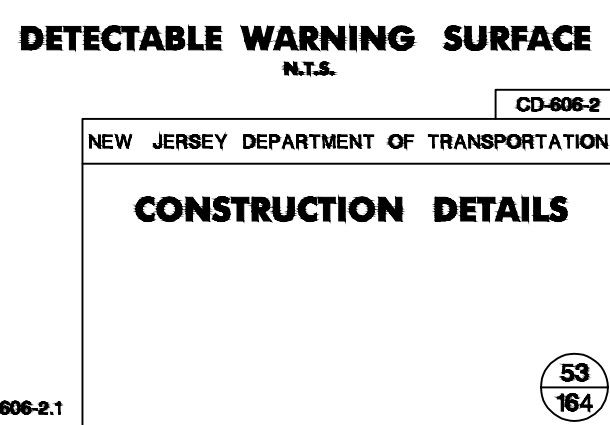
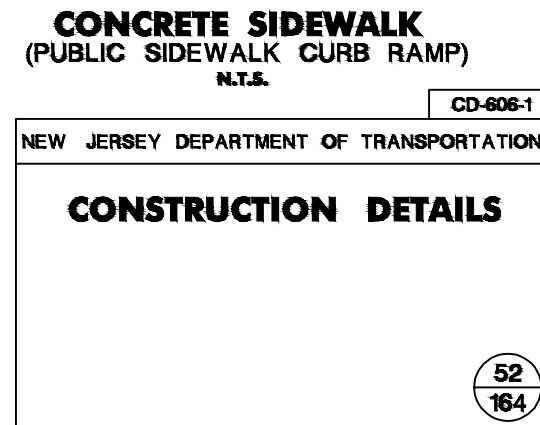
**DUNKIN'**


LOT 64 BLOCK 28005  
MONTGOMERY TOWNSHIP  
SOMERSET COUNTY  
NEW JERSEY

SHEET  
12 OF 13

DATE: 2/25/2021 PROJECT NO:  
REVISION: 141046





10.00	4.41	23.41	10.00	3.24	22.24
<b>CONCRETE SIDEWALK</b>					
<b>(PUBLIC SIDEWALK CURB RAMP TABLES)</b>					
<b>N.T.S.</b>					
					<b>CD-608-3</b>
<b>NEW JERSEY DEPARTMENT OF TRANSPORTATION</b>					
<b>CONSTRUCTION DETAILS</b>					
					

**CONCRETE SIDEWALK**  
**G SIDEWALK CURB RAMP TABLES)**  
 N.T.S.

CD-608-4

NEW JERSEY DEPARTMENT OF TRANSPORTATION

**CONSTRUCTION DETAILS**

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